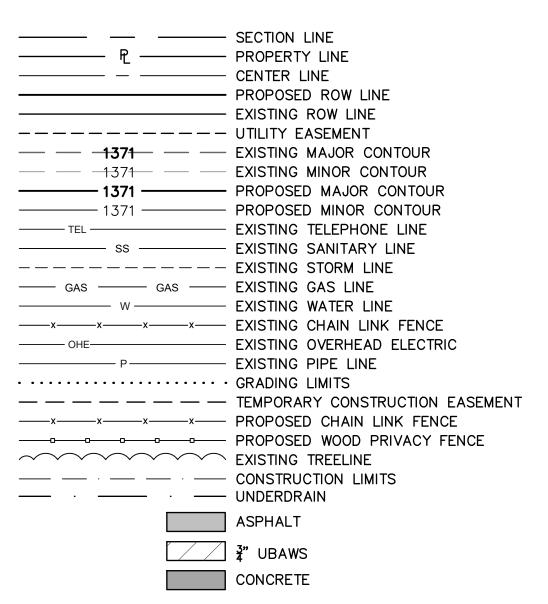
PUBLIC STREET PLANS FOR

US HIGHWAY 50 (BROADWAY BOULEVARD) PUBLIC IMPROVEMENTS

LEGEND

≜ CTL	SURVEY CONTROL POINT	TS	TRAFFIC SIGNAL BOX
	SURVEY BENCHMARK	(19)	TRAFFIC SIGNAL MANHOLE
<u></u> В ТВМ	SURVEY TEMPORARY BEMCHMARK	0-000	TRAFFIC SIGNAL POLE W/ ARM
	GAS METER	000	TRAFFIC SIGNAL POLE
GAR	GAS RISER	TO	TRAFFIC SIGNAL CONTROL BOX
©	GAS MANHOLE	TS	TRAFFIC SIGNAL PEDESTAL
☐GR	GAS REGULATOR	(E)	ELECTRIC MANHOLE
TVP	TELEVISION PEDESTAL	EM	ELECTRIC METER
F	FIBER BOX	ER	ELECTRIC RISER
©	FIBER PEDESTAL	E	ELECTRIC BOX
С	CABLE BOX	\bowtie	ELECTRIC CABINET
CV	CABLE VAULT	J	ELECTRIC JUNCTION BOX
TP	TELEPHONE PEDESTAL	○SPH	SPRINKLER HEAD
(D)	STORM MANHOLE	⋈scv	SPRINKLER CONTROL VALVE
	STORM GRATE	MP	WATER METER PIT
S	SANITARY MANHOLE	- Ó	FIRE HYDRANT
ΦYL	YARD LIGHT	WM	WATER METER
ФLТР	LIGHT POLE	⊠wv	WATER VALVE
	POWER POLE	OFP	FLAG POLE
 HLPPP	POWER POLE W/ LIGHT	$\overline{}$	SIGN
\leftarrow	GUY WIRE	\circledast	BOLLARD
P\ BU	STUMP	\blacksquare	WOOD POST
\odot	BUSH	₩	STEEL POST
W.	EVERGREEN TREE	OCOL	COLUMN
\odot	DECIDUOUS TREE	•	BORE HOLE



ABBREVIATION TABLE

MATCH GRADE PAVEMENT TOP OF CURB BACK OF CURB EDGE OF PAVEMENT EΡ RIGHT-OF-WAY TEMPORARY CONSTRUCTION EASEMENT PROPOSED EXIST. **EXISTING** TYP. **TYPICAL** REMOVAL CONST. CONSTRUCT TEMPORARY BENCMARK CONTROL POINT DO NOT DISTURB USE IN PLACE ADJUST ELEVATION ESMT. EASEMENT



THE EXISTING UTILITY LOCATIONS SHOWN ON THE PLANS ARE APPROXIMATE AND MAY NOT INCLUDE ALL LINES PRESENT. THE CONTRACTOR SHALL BE RESPONSIBLE TO CALL "1-800-DIG-RITE", AND COORDINATE FIELD LOCATION OF EXISTING UNDERGROUND UTILITIES PRIOR TO BEGINNING GRADING ACTIVITIES. !!STOP!! CALL BEFORE YOU DIG!!

CITY OF SEDALIA, MISSOURI PETTIS COUNTY, MISSOURI



INDEX OF SHEETS

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STORM SYSTEM PLAN & PROFILES **EROSION CONTROL**

11-20 STANDARD DETAILS

PAVEMENT MARKING & SIGNING PLAN 21-22

TRAFFIC CONTROL PLAN **CROSS SECTIONS**

N.T.S.

UTILITY COMPANIES

NOT TO SCALE

TELEPHONE SERVICE
SOCKET TELECOM **EVAN CAMPBELL** 2703 CLARK LN. COLUMBIA, MO 65202 (573) 447-7575

GAS SERVICE AMY WOOLRIDGE (573) 979-5015

STORM WATER FACILITIES
CITY OF SEDALIA PRETREATMENT COORDINATOR BRITTANY WILLBANKS 200 SOUTH OSAGE AVE. SEDALIA, MO 65301 (660) 620-5160

WATER SERVICE WATER DEPARTMENT DAVID MURRAY 200 SOUTH OSAGE AVE. SEDALIA, MO 65301 (660) 460-0118

SANITARY SEWER SERVICE CITY OF SEDALIA DAVID GURKIN 200 SOUTH OSAGE AVE. SEDALIA, MO 65301 (660) 221-8824

ELECTRIC SERVICE EVERGY 1710 PASEO BLVD. KANSAS CITY, MO 64108 (888) 544-4852

7301 West 133rd Street Suite 200 Overland Park, KS 66213 TEL 913.381.1170 FAX 913.381.1174 Missouri COA #001592



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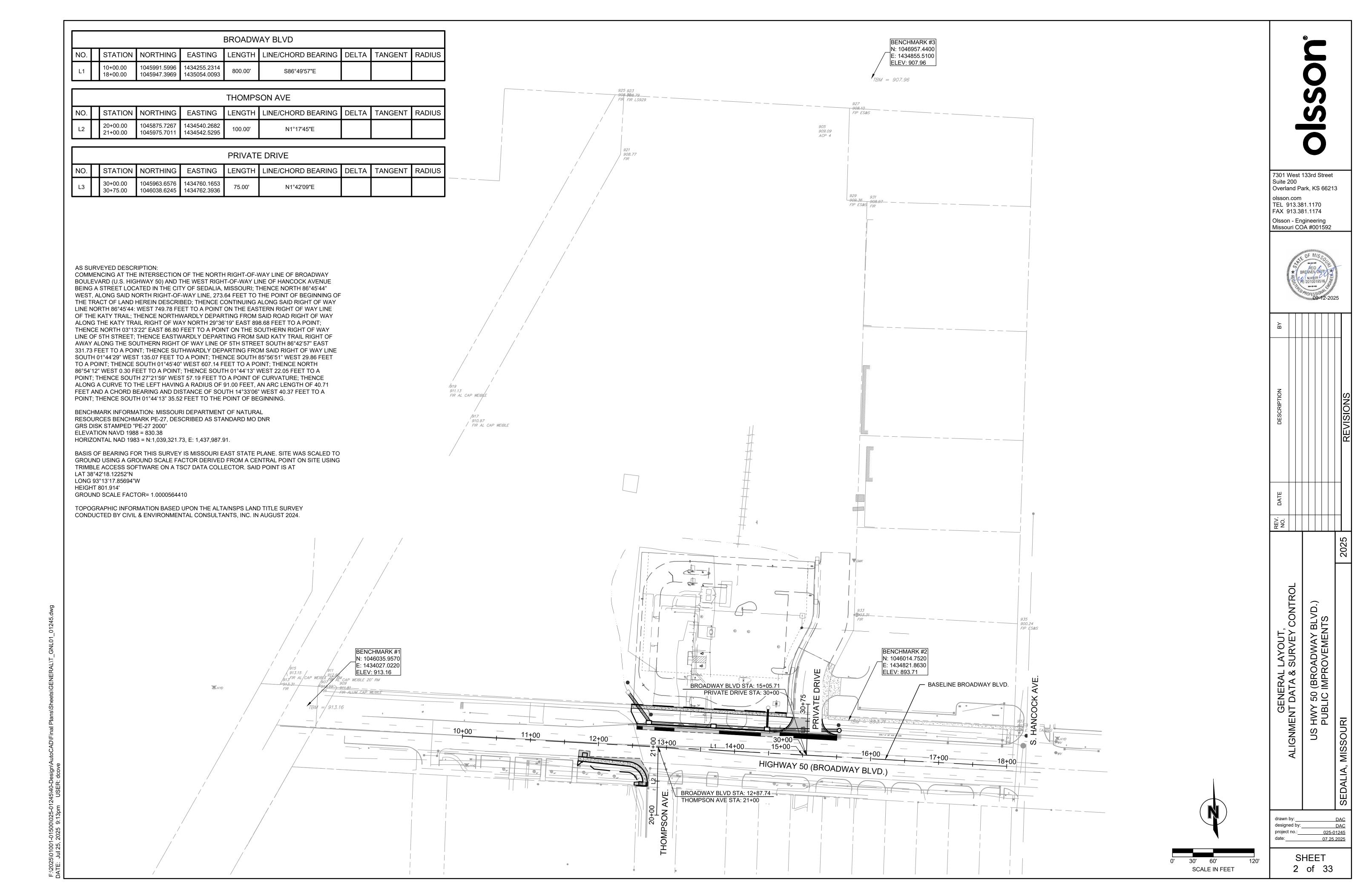
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SHEET 1 of 33

PREPARED & SUBMITTED BY: OLSSON, INC

7301 W. 133RD STREET, SUITE 200

OVERLAND PARK, KANSAS 66213



GENERAL NOTES:

- 1. THE CONTRACTOR SHALL HAVE ONE (1) SIGNED COPY OF THE PLANS (APPROVED BY THE CITY OF SEDALIA, MO.) AND (1) COPY OF THE APPROPRIATE DESIGN AND CONSTRUCTION STANDARDS AND SPECIFICATIONS AT THE JOB SITE AT ALL TIMES.
- 2. THE LATEST EDITION OF MoDOT STANDARDS AND SPECIFICATIONS SHALL GOVERN CONSTRUCTION OF THE PROJECT.
- 3. THE CONTRACTOR SHALL BE RESPONSIBLE FOR OBTAINING ALL REQUIRED PERMITS, OTHERWISE COMPLYING WITH ALL APPLICABLE REGULATIONS GOVERNING THE WORK.
- 4. CONSTRUCTION OF THE IMPROVEMENTS SHOWN OR IMPLIED BY THIS SET OF DRAWINGS SHALL NOT BE INITIATED OR ANY PART THEREOF UNDERTAKEN UNTIL THE DIRECTOR OF PUBLIC WORKS OR HIS AGENT IS NOTIFIED OF SUCH INTENT AND ALL REQUIRED PERMITS, PROPERTY ACQUISITIONS, SECURITY AND OTHER CONTRACT BONDS, AND CONTRACT AGREEMENTS ARE RECEIVED AND APPROVED BY THE CITY.
- 5. PRIOR TO ORDERING PRECAST STRUCTURES, SHOP DRAWINGS ARE TO BE SUBMITTED TO THE DESIGN ENGINEER FOR APPROVAL.
 THE DESIGN ENGINEER SHALL INDICATE APPROVAL OF THE SHOP DRAWINGS AND ADD THE PERMIT TYPE AND NUMBER ON THEM.



- 6. ALL EXISTING UTILITIES INDICATED ON THE DRAWINGS ARE ACCORDING TO THE BEST INFORMATION AVAILABLE TO THE ENGINEER; HOWEVER ALL UTILITIES ACTUALLY EXISTING MAY NOT BE SHOWN. UTILITIES DAMAGED THROUGH THE NEGLIGENCE OF THE CONTRACTOR TO OBTAIN THE EXACT LOCATION OF SAME SHALL BE REPAIRED OR REPLACED BY THE CONTRACTOR AT THEIR EXPENSE. ONE-CALL/DIG RITE (1-800-344-7483 AND 1-800-DIG-RITE).
- 7. ALL PRIVATELY OWNED UTILITIES IN CONFLICT WITH THESE IMPROVEMENTS SHALL BE RELOCATED BY THE APPROPRIATE UTILITY COMPANIES. UTILITY RELOCATIONS WILL BE COMPLETED PRIOR TO THE START OF CONSTRUCTION, WHEN PRACTICABLE.
- 8. CONSTRUCTION STAKING IS THE RESPONSIBILITY OF THE CONTRACTOR. THE CONTRACTOR SHALL SET THOSE STAKES NECESSARY TO CONSTRUCT THIS PROJECT IN ACCORDANCE WITH THE PLANS.
- 9. CONTRACTOR SHALL REMOVE THOSE TREES NECESSARY TO ACCOMPLISH THE GRADING WORK SHOWN HEREON. ALL TREES AND BRUSH FROM THE WORK SHALL BE COMPLETELY REMOVED FROM THE SITE BY THE CONTRACTOR. BURNING WILL NOT BE ALLOWED ON THE PROJECT.
- 10. ALL BACKFILL SHALL BE PLACED AND COMPACTED TO THE REQUIRED DENSITY AS INDICATED IN THE SPECIFICATIONS.
- 11. ALL ROADWAY EXCAVATION IN ROCK (SHALE, SANDSTONE OR LIMESTONE) WILL BE UNDERCUT NO LESS THAN 6" INCHES FOR THE FULL WIDTH OF THE ROADWAY AND BACKFILLED WITH SUITABLE SOIL OR GRANULAR MATERIAL. THERE WILL BE NO DIRECT PAYMENT FOR ROCK EXCAVATION. PAYMENT FOR ALL MATERIAL WILL BE MADE AT THE CONTRACT BID PRICE FOR UNCLASSIFIED EXCAVATION.
- 12. THE CONTRACTOR SHALL USE ADEQUATE DUST CONTROL MEASURES DURING ALL PHASES OF THE CONSTRUCTION TO MINIMIZE DUST ON SURROUNDING PROPERTIES AT THE CONTRACTOR'S EXPENSE.
- 13. DRIVEWAYS, SIDEWALKS, FENCES, SPRINKLER SYSTEMS AND OTHER AREAS DAMAGED BY THE CONTRACTOR SHALL BE RESTORED TO A CONDITION EQUAL TO OR BETTER THAN THAT EXISTING BEFORE DAMAGE OCCURRED. THIS WORK WILL NOT BE PAID FOR DIRECTLY, BUT IS CONSIDERED SUBSIDIARY TO OTHER BID ITEMS.

- 14. WHERE THE NEW STREET IS TO CONNECT TO AN EXISTING STREET ALL DETERIORATED OR CRACKED ASPHALT WITHIN TWO (2) FEET OF THE CONNECTION POINT SHALL BE REMOVED TO A DEPTH WHERE SOUND MATERIAL IS FOUND. IF FULL-DEPTH PAVEMENT REMOVAL IS REQUIRED, THE SUB-GRADE SHALL BE RE-COMPACTED TO A MINIMUM OF 95% OF STANDARD PROCTOR DENSITY (ASTM D-698).
- 15. ALL EXISTING FENCING, DRIVEWAYS, SIDEWALKS AND OTHER ITEMS WITHIN THE CONSTRUCTION LIMITS DAMAGED OR REMOVED BY THE CONTRACTOR, SHALL BE REPLACED WITH NEW MATERIALS. ALL FENCE REPLACEMENT SHOULD BE RELOCATED TO THE PROPERTY LINES UNLESS THE EXISTING FENCE WAS LOCATED INSIDE THE PROPERTY LINES, THEN THE FENCE SHOULD BE REPLACED IN ITS ORIGINAL LOCATION. FENCE CORNERS OF FENCES TO BE RELOCATED AT THE ORIGINAL LOCATIONS SHALL BE REFERENCE OUT BY THE CONTRACTOR BEFORE REMOVAL. REMOVALS SHALL BE AS SHOWN ON THESE PLANS, OR APPROVED BY THE CITY PRIOR TO REMOVAL. REMOVALS NOT PRE-APPROVED WILL BE REPLACED AT THE CONTRACTOR'S EXPENSE.
- 16. REMOVAL OF EXISTING STORM DRAINAGE STRUCTURES AND OTHER EXISTING PHYSICAL FEATURES OF THE EXISTING ROADWAY SHALL BE CONSIDERED INCIDENTAL AND REMOVED AS A <u>SUBSIDIARY</u> ITEM TO CLEARING, GRUBBING, DEMOLITION, & REMOVALS. ALL WASTE SHALL BE TRANSPORTED TO A CITY APPROVED DUMP SITE FOR DISPOSAL.
- 17. ALL EXISTING WATER VALVES, METERS, FIRE HYDRANTS AND BLOW-OFF ASSEMBLIES WITHIN THE LIMITS OF THE PROPOSED PAVEMENT SHALL BE RELOCATED OR ADJUSTED TO GRADE BY THE CONTRACTOR. WATER METERS SHALL BE RELOCATED SO THAT THEY ARE WITHIN THE PUBLIC RIGHT-OF -WAY, APPROXIMATELY ONE FOOT FROM RIGHT-OF WAY LINE. WATER SERVICE LINES AFFECTED BY METER RELOCATION AND/OR THE WATER MAIN RELOCATIONS SHOWN IN THESE PLANS SHALL BE RECONNECTED BY THE CONTRACTOR. THE CONTRACTOR SHALL NOTIFY RESIDENTS OF ANY WATER SERVICE INTERRUPTIONS 48 HOURS PRIOR TO INTERRUPTING THE SERVICE. ANY SERVICE INTERRUPTIONS MUST BE RESTORED THE SAME DAY.
- 18. THE CONTRACTOR SHALL BE RESPONSIBLE FOR PRESERVING PROPERTY IRONS. THE CONTRACTOR SHALL BE REQUIRED TO RE-ESTABLISH ANY PROPERTY IRONS AND SURVEY MONUMENTS WHICH HAVE BEEN DAMAGED OR DESTROYED BY THIS CONSTRUCTION OPERATION INCLUDING THOSE BURIED WITH EMBANKMENT OR REMOVED DURING EXCAVATION. SUCH IRONS AND MONUMENTS SHALL BE RE-ESTABLISHED BY A LICENSED LAND SURVEYOR. REPLACEMENT OF PROPERTY IRONS AND MONUMENTS SHALL BE SUBSIDIARY TO OTHER BID ITEMS.
- 19. THE TOP 6" OF ALL DISTURBED AREAS THAT ARE TO BE SEEDED OR SODDED SHALL BE OF MATERIAL SUITABLE FOR SUSTAINING GRASS. THE GRADED SURFACE SHALL BE MADE FREE OF ROCK, CONCRETE, BRICK, OR FRAGMENTS THEREOF, OR RUBBISH AND SHALL BE FINISHED TO THE LINES, GRADES, AND CROSS-SECTION INDICATED ON THE PLANS.
- 20. ALL WATERLINE MAINS, SANITARY SEWER MAINS AND STORM WATER DRAINAGE CROSSINGS UNDER PROPOSED STREET PAVEMENTS SHALL BE IN PLACE PRIOR TO THE PLACEMENT OF THE ASPHALT SURFACE OR BASE COURSES.
- 21. APPROPRIATE TRAFFIC CONTROL DEVICES, SIGNAGE, AND PAVEMENT MARKINGS SHALL BE ESTABLISHED AND MAINTAINED THROUGHOUT THE PROJECT IN ACCORDANCE WITH THE CONTRACT DOCUMENTS AND MUTCO.
- 22. HANDICAP RAMPS SHALL BE CONSTRUCTED IN ACCORDANCE WITH ADA REQUIREMENTS AT ALL LOCATIONS WHERE POSSIBLE.
- 23. THE CONTRACTOR MUST REMOVE AT THEIR COST ANY BAD SUBSURFACE SOIL WHICH WOULD NOT BE ABLE TO SUPPORT ANY PROPOSED PUBLIC IMPROVEMENT. BACKFILL SHOULD BE ACCOMPLISHED IN ACCORDANCE WITH THE LATEST EDITION OF MoDOT
- STANDARDS AND SPECIFICATIONS.

 24. ALL SIGNS INSTALLED, INCLUDING STOP SIGNS, KEEP RIGHT SIGNS AND OBJECT MARKERS ALONG WITH THE SIGN POSTS SHALL CONFORM TO THE LATEST EDITION OF THE MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES (MUTCD) AND APWA GUIDELINES.
- 25. SAW CUTS SHALL BE FULL DEPTH. THIS SHALL BE <u>SUBSIDIARY</u> TO PAVING
- 26. ALL EXISTING STRUCTURES WITHIN THE CONSTRUCTION LIMITS SHALL BE REMOVED, UNLESS OTHERWISE NOTED ON THE PLANS. THIS WORK SHALL BE PAID FOR UNDER THE BID ITEM "REMOVAL OF EXISTING STRUCTURES".
- 27. THE CONTRACTOR SHALL AT NO TIME LEAVE EQUIPMENT, MATERIALS OR DEBRIS WITHIN 4' OF TRAVELED WAY OR LOCATIONS THAT COULD OBSTRUCT INTERSECTION SIGHT DISTANCE, OBSTRUCT ANY EXISTING CAPACITY OF STORM SEWER SYSTEM, OR CAUSE FLOODING OR EROSION TO RESIDENCES.
- 28. THE CONTRACTOR SHALL POTHOLE AND SURVEY ALL UTILITY CROSSINGS PRIOR TO CONSTRUCTION OF ANY PORTION OF STORM SEWER, UNDERDRAINS, CONDUIT, AND ANY OTHER SUBSURFACE ELEMENTS OF THE PROJECT. THIS SURVEY INFORMATION SHALL BE FORWARDED TO THE ENGINEER FOR REVIEW. THE CONTRACTOR SHALL NOT BEGIN CONSTRUCTION ON ANY SUBSURFACE ELEMENT ON THE PROJECT WITHOUT THE APPROVAL OF THE ENGINEER. THIS ITEM SHALL BE SUBSIDIARY TO OTHER BID ITEMS.
- 30. THE CONTRACTOR SHALL SUPPORT AND PROTECT ALL EXPOSED UTILITIES IN TRENCHES AND BRACE ALL POLES AS REQUIRED BY THE ENGINEER.
- 31. REINFORCED CONCRETE PIPE SHALL BE CLASS III, UNLESS OTHERWISE NOTED ON THE PLANS.
- 32. CONTACT THE MISSOURI DEPARTMENT OF TRANSPORTATION PERMITS AT 816-607-2187 AT LEAST 24 HOURS PRIOR TO BEGINNING WORK AND FOR REQUIRED INSPECTIONS ON ORTIONS IN THE MoDOT RIGHT OF WAY.
- 33. ANY WORK PERFORMED WITHIN MoDOT RIGHT OF WAY SHALL CONFORM TO MoDOT STANDARDS AND SPECIFICATIONS. IN ADDITION, THE CONTRACTOR SHALL PROVIDE MATERIALS CERTIFICATION AND COMPACTION TESTING THAT COMPLIES WITH MoDOT SPECIFICATIONS.
- 34. CONTRACTOR SHALL CONTACT ENGINEER FOR STRUCTURAL DETAILS ON INLET C1 PRIOR TO DEMOLITION AND ONCE DIMENSIONS ARE KNOWN.
- 35. STORM STRUCTURES SHALL BE PRECAST (OTHER THAN C1) WITH BOLT DOWN LIDS (ALL STRUCTURES).

BEST MANAGEMENT PRACTICES:

1. PRIOR TO LAND DISTURBANCE ACTIVITIES, THE CONTRACTOR SHALL

A. PRIOR TO BEGINNING EXCAVATION, THE CONTRACTOR SHALL INSTALL SILT FENCE ALONG THE CONSTRUCTION LIMITS TO DIVERT CLEAN WATER AWAY FROM THE DISTURBED SITE. THE CONTRACTOR AND THE ENGINEERS REPRESENTATIVE SHALL USE DISCRETION IN THE PLACEMENT OF THE SILT FENCE AS TO PREVENT FLOODING IN UNDESIRABLE LOCATIONS.

B. CONSTRUCT A STABILIZED ENTRANCE/PARKING/DELIVERY AREA AND INSTALL ALL PERIMETER SEDIMENT CONTROLS ON THE SITE.

C. INSTALL AND REQUEST THE INSPECTION OF THE PRE-CONSTRUCTION EROSION AND SEDIMENT CONTROL MEASURES DESIGNATED ON THE APPROVED EROSION AND SEDIMENT CONTROL PLAN. LAND DISTURBANCE WORK SHALL NOT PROCEED UNTIL THERE IS A SATISFACTORY INSPECTION.

D. TEMPORARY FENCING SHALL BE INSTALLED ALONG ALL TEMPORARY EASEMENTS AND MAINTAINED FOR THE DURATION OF THE PROJECT.

- 2. THE CONTRACTOR SHALL MINIMIZE THE AREA DISTURBED DURING EXCAVATION AND CONSTRUCTION OF THE STORM SEWER PIPES AND STRUCTURES. FOLLOWING THE CONSTRUCTION OF THE STORM SEWER LINE, THE CONTRACTOR SHALL INSTALL THE TEMPORARY DITCH CHECKS AT 100' INTERVALS WITHIN TWO WORKING DAYS AFTER CONSTRUCTION ACTIVITY HAS CEASED IN THAT AREA. THE TEMPORARY DITCH CHECKS SHALL REMAIN IN PLACE UNTIL SOD HAS BEEN PLACED ON THE DISTURBED AREAS. INLETS SHALL BE PROTECTED WITH SEDIMENT BARRIERS AS SHOWN ON THE DETAIL.
- 3. THE CONTRACTOR SHALL PERFORM INSPECTIONS OF EROSION AND SEDIMENT CONTROL MEASURES AT LEAST ONCE PER WEEK AND WITHIN 24 HOURS FOLLOWING EACH RAINFALL EVENT OF 1/2" OR MORE WITHIN ANY 24-HOUR PERIOD.
- 4. THE CONTRACTOR SHALL MAINTAIN AN INSPECTION LOG INCLUDING THE INSPECTOR'S NAME, DATE OF INSPECTION, OBSERVATIONS AS TO THE EFFECTIVENESS OF THE EROSION AND SEDIMENT CONTROL MEASURES, ACTIONS NECESSARY TO CORRECT DEFICIENCIES, WHEN THE DEFICIENCIES WERE CORRECTED, AND THE SIGNATURE OF THE PERSON PERFORMING THE INSPECTION. THE LOG SHALL BE AVAILABLE FOR REVIEW BY THE CITY OF SEDALIA AND THE STATE OF MISSOURI.
- 5. THE CONTRACTOR SHALL MAINTAIN INSTALLED EROSION AND SEDIMENT CONTROL DEVICES IN A MANNER THAT PRESERVES THEIR EFFECTIVENESS FOR PREVENTING SEDIMENT FROM LEAVING THE SITE OR ENTERING A SENSITIVE AREA SUCH AS A NATURAL STREAM CORRIDOR, AREAS OF THE SITE INTENDED TO BE LEFT UNDISTURBED, A STORM SEWER, OR AN ON-SITE DRAINAGE CHANNEL.
- 6. THE CONTRACTOR IS RESPONSIBLE FOR PROVIDING EROSION AND SEDIMENT CONTROL FOR THE DURATION OF A PROJECT. IF MoDOT OR THE CITY DETERMINES THAT THE BMP'S IN PLACE DO NOT PROVIDE ADEQUATE EROSION AND SEDIMENT CONTROL AT ANY TIME DURING THE PROJECT, THE CONTRACTOR SHALL INSTALL ADDITIONAL OR ALTERNATE MEASURES THAT PROVIDE EFFECTIVE CONTROL.
- 7. EXCAVATED MATERIAL AND TOPSOIL STAGING SHALL NOT BE PERMITTED IN STREET RIGHT-OF-WAY AND SHALL BE CONFINED TO DRAINAGE EASEMENTS AND THOSE AREAS IDENTIFIED ON THIS PLAN. STOCKPILE AREAS SHALL BE PROTECTED WITH SILT FENCING TO PREVENT SOIL LOSS TO NEARBY STORM DRAINAGE SYSTEMS AND PROPERTY
- 8. PROJECT SHALL BE CONSTRUCTED DOWNSTREAM TO UPSTREAM. THE AMOUNT OF DISTURBED AREA LEFT EXPOSED SHALL BE MINIMIZED.
- ALL AREA INLETS AND CURB INLETS SHALL BE PROTECTED WITH TEMPORARY INLET SEDIMENT BARRIERS PRIOR TO TOP SLAB INSTALLATION AS WELL AS AFTER TOP SLAB INSTALLATION UNTIL SODDING AND PAVING IS COMPLETE.
- 10. SODDING SHALL OCCUR IN PHASES AS DEPICTED ON THIS PLAN IN ACCORDANCE TO THE LATEST EDITION OF MoDOT STANDARDS AND SPECIFICATIONS.

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GENERAL NOTES

HWY 50 (BROADWAY BLVD.)

PUBLIC IMPROVEMENTS

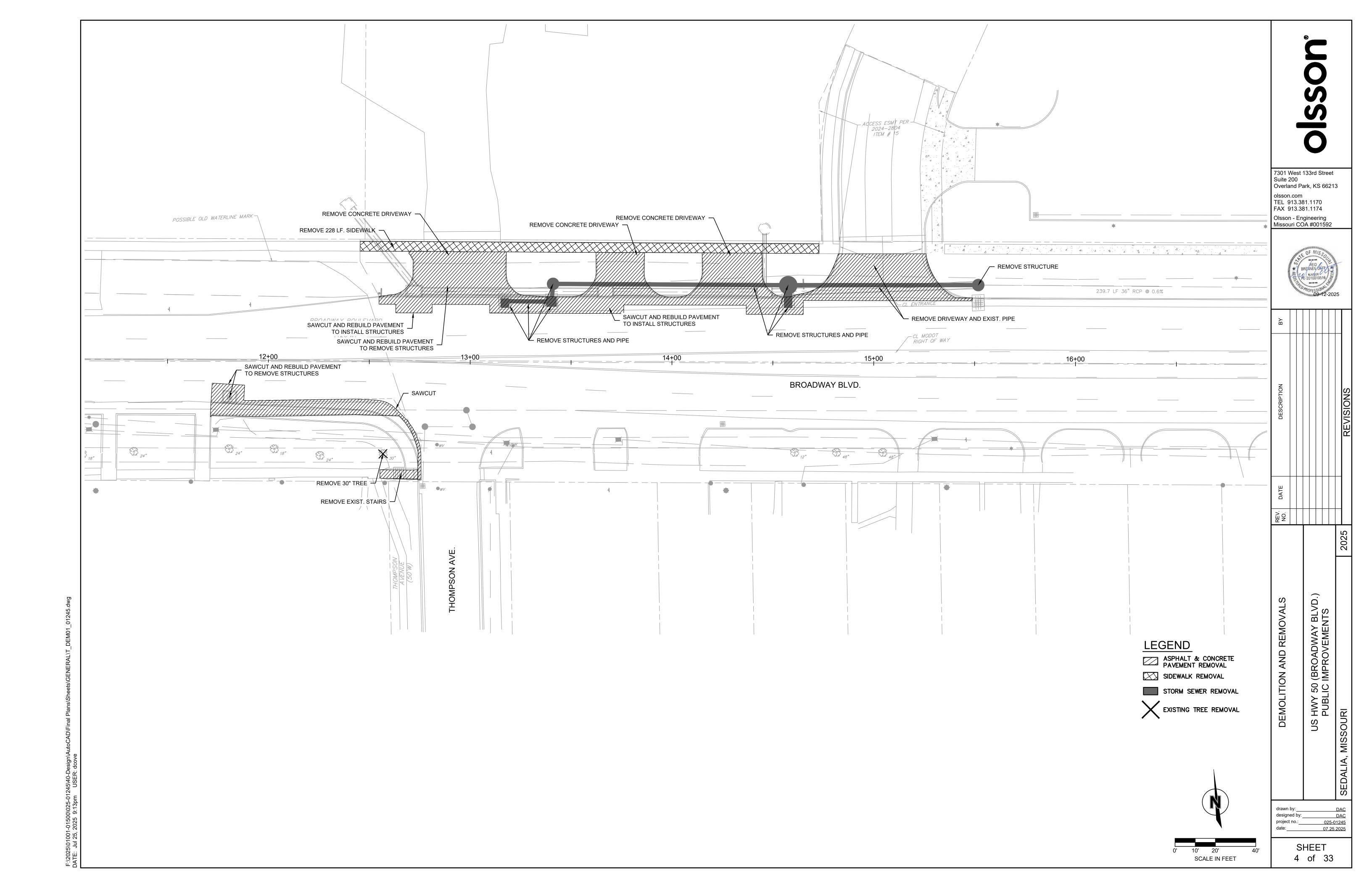
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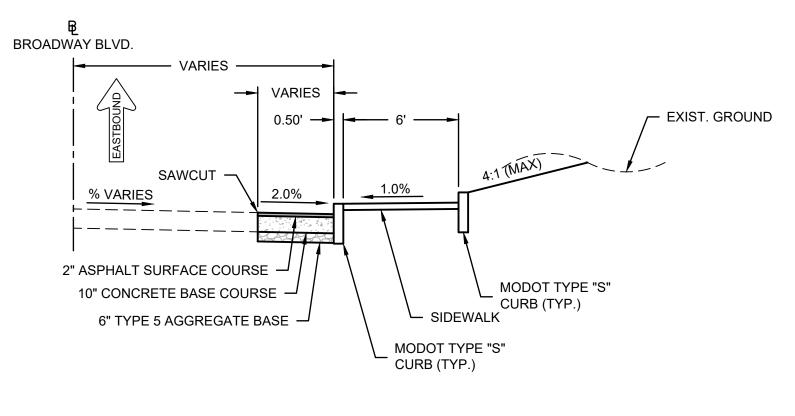


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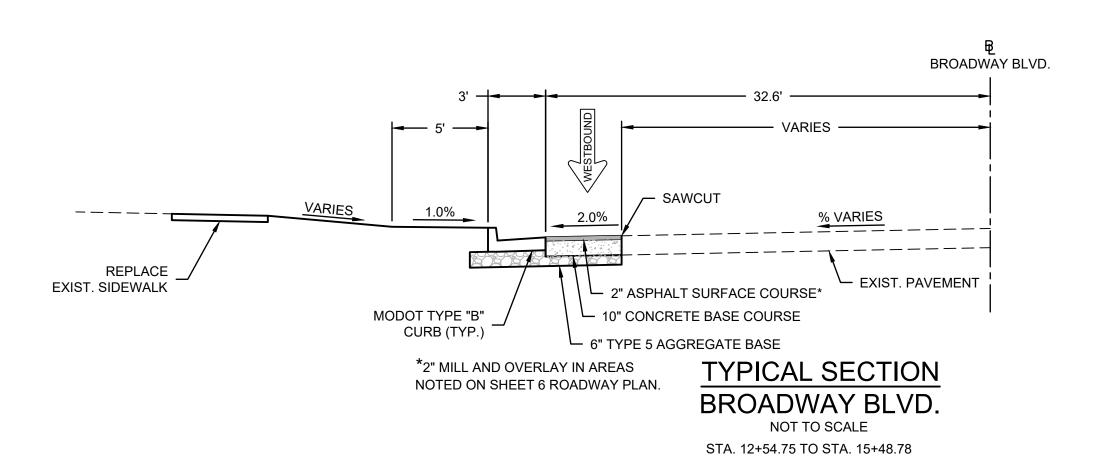
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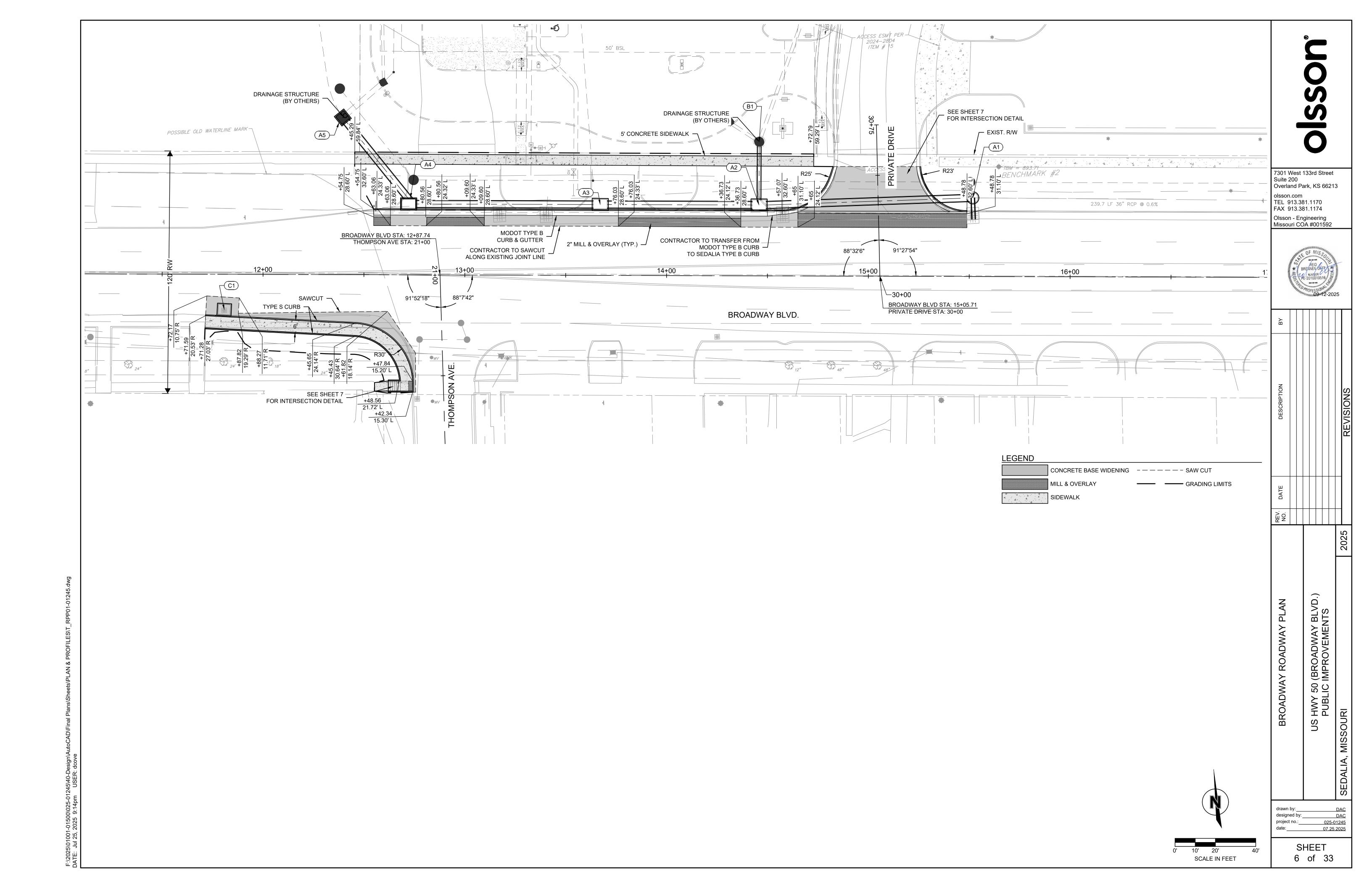
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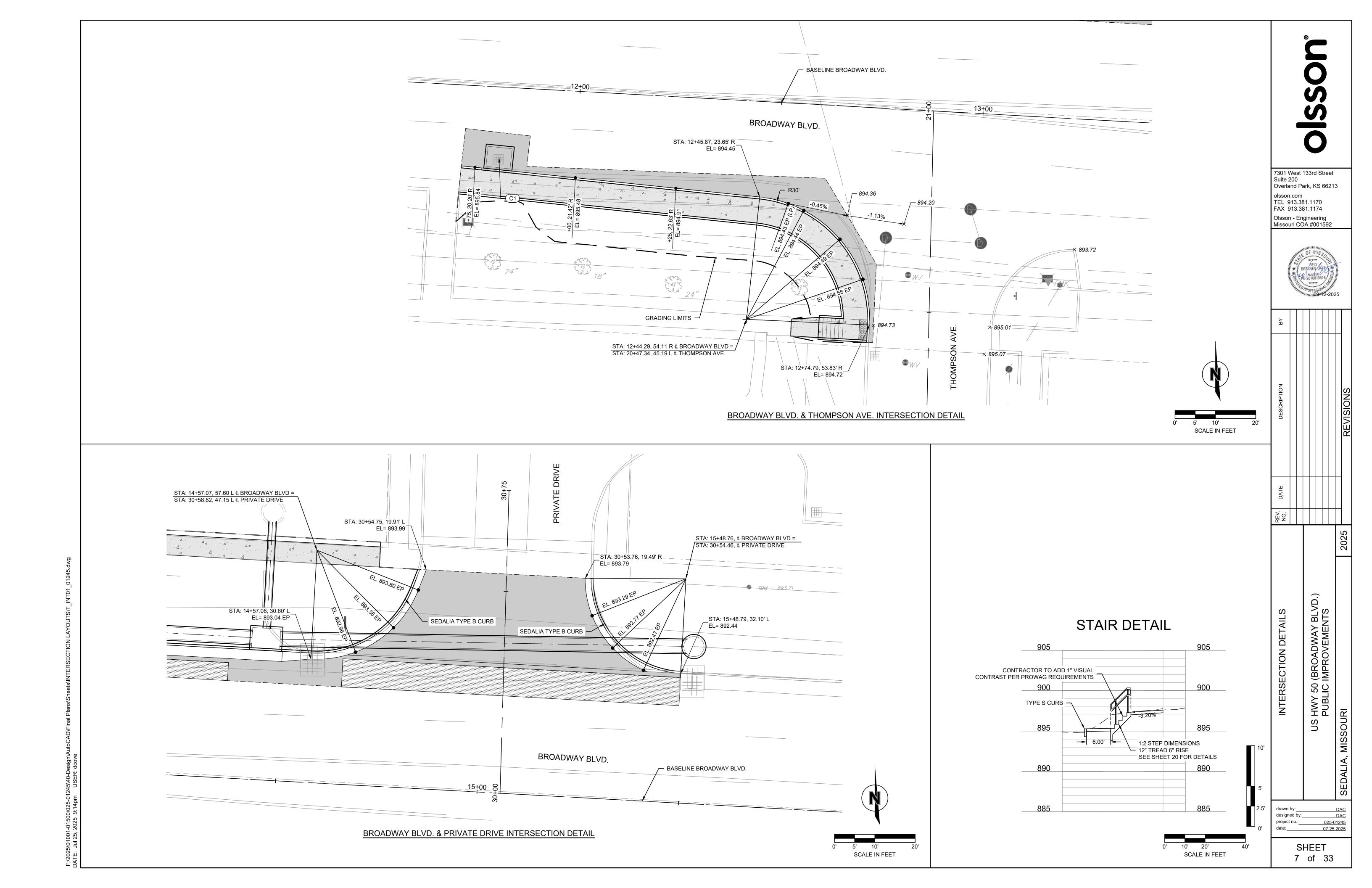


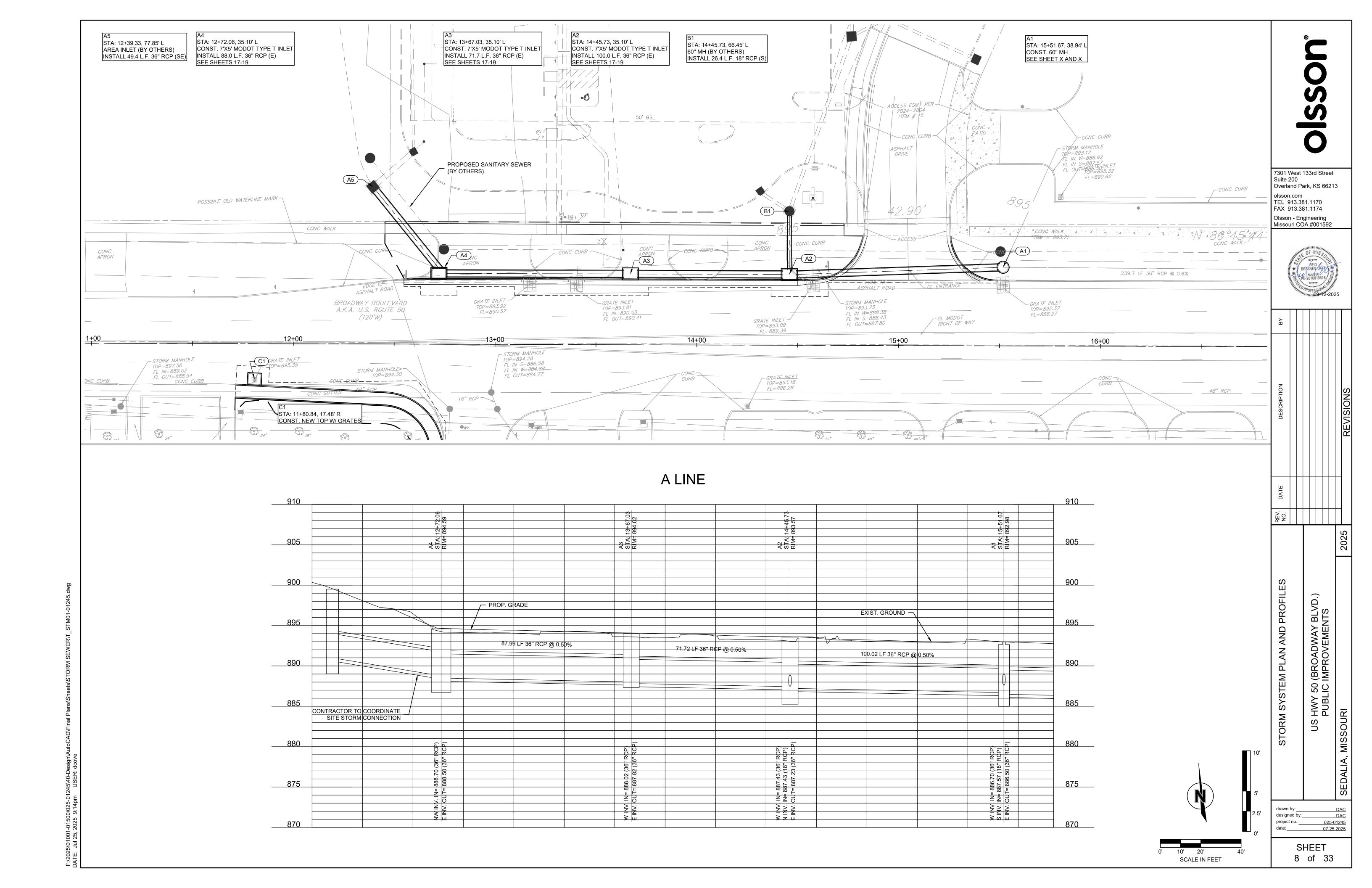
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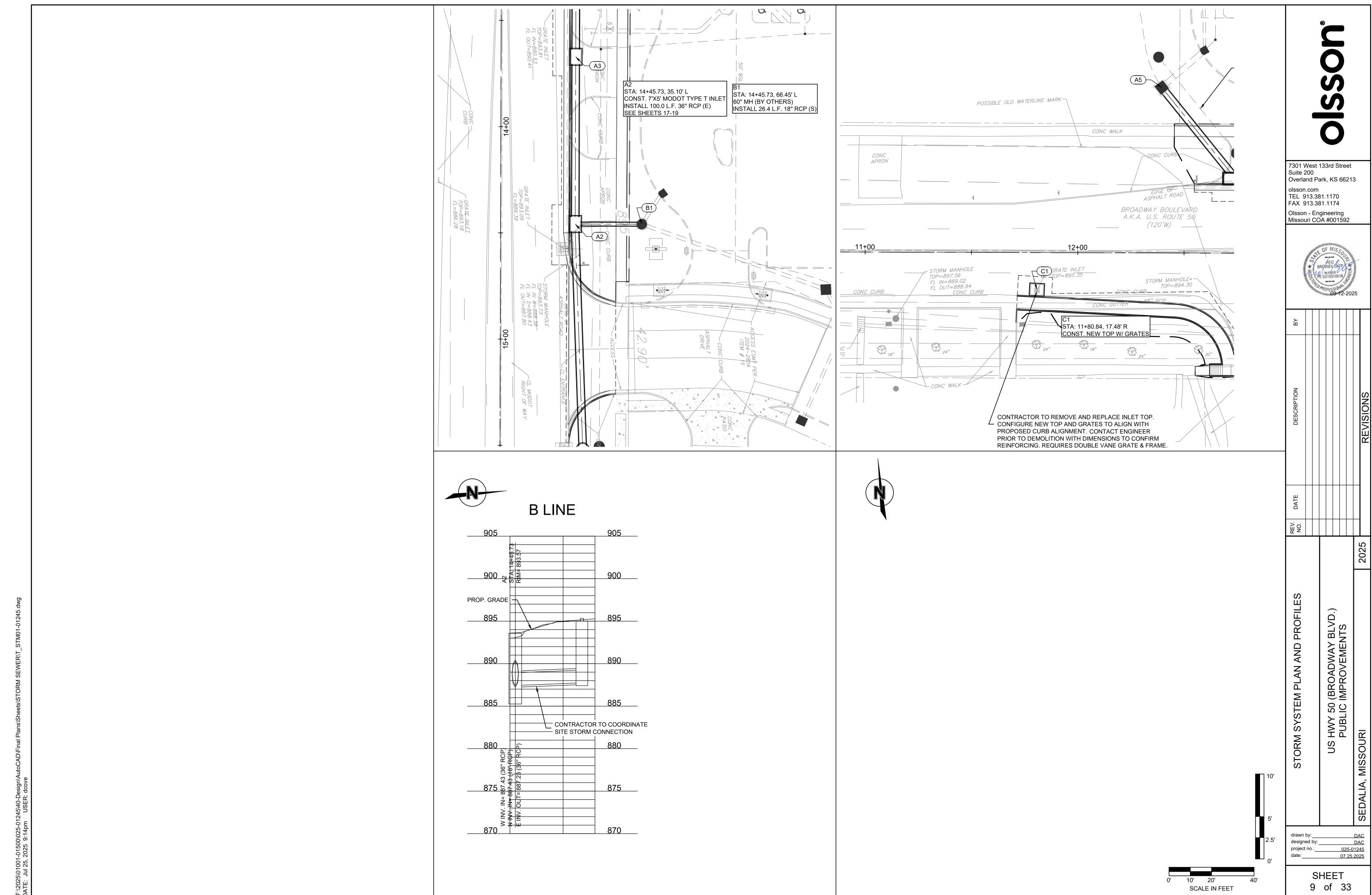
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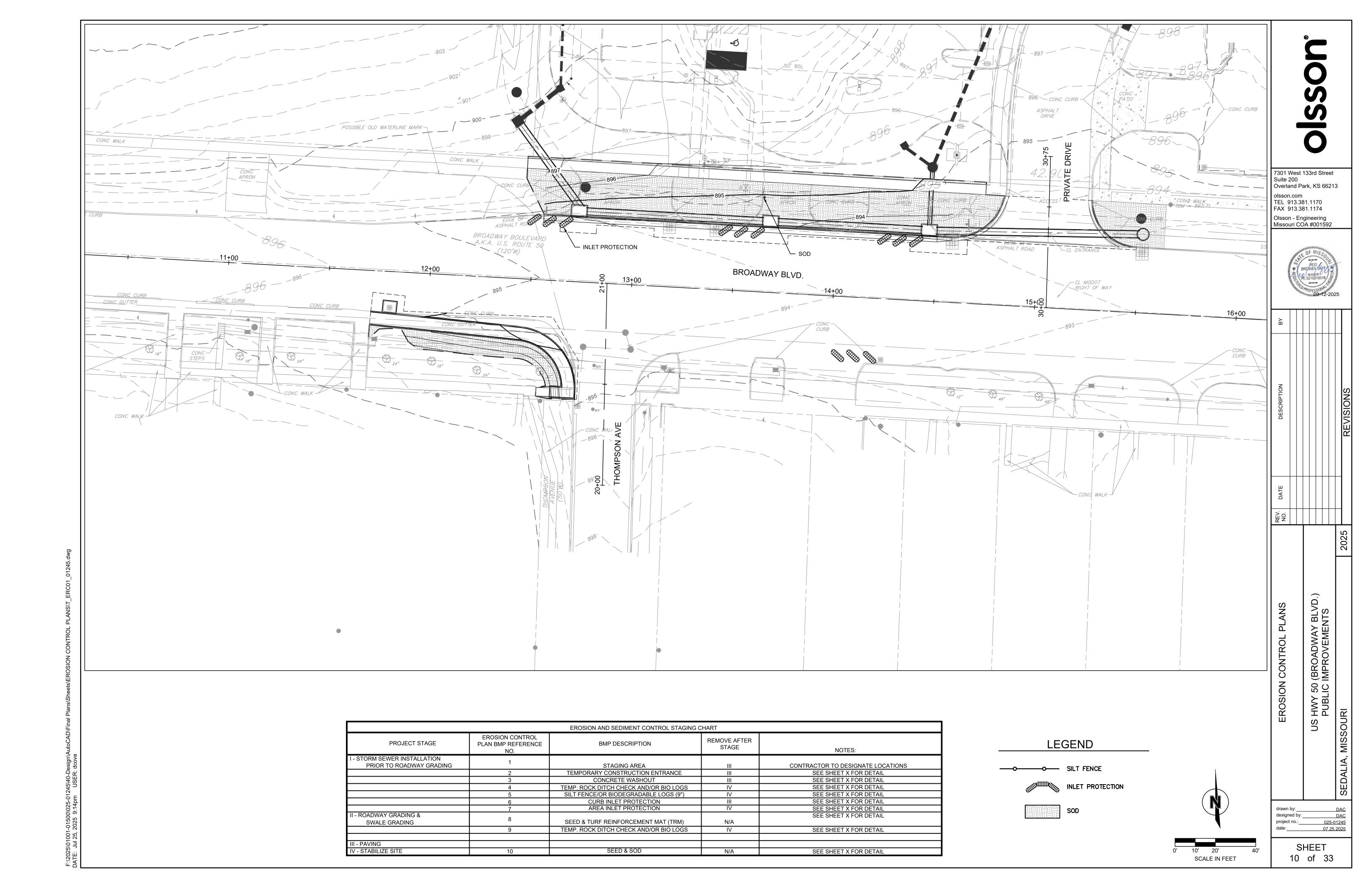


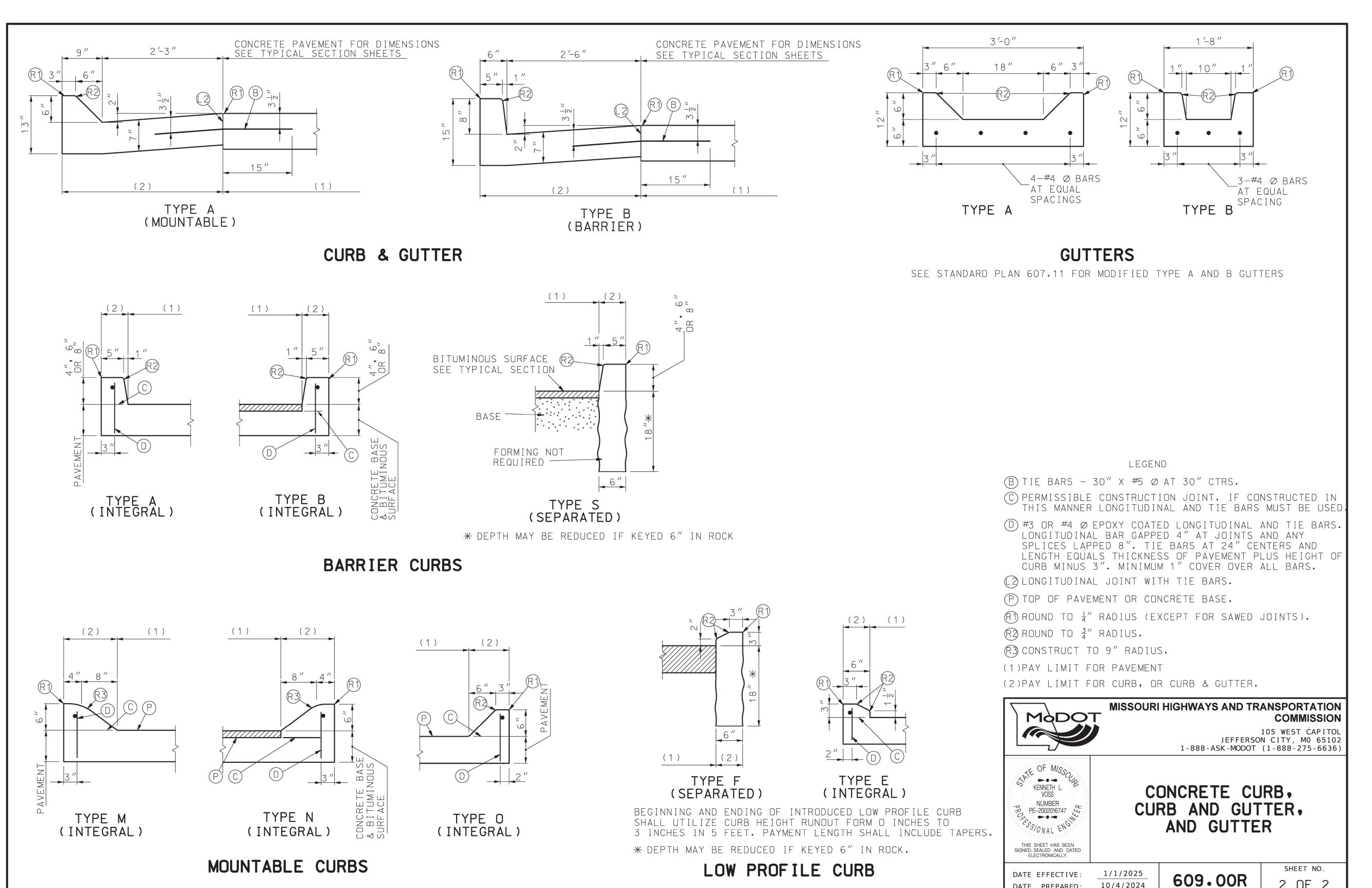














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US HWY 50 (BROADWAY BLVD.) PUBLIC IMPROVEMENTS

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1'-6" *0000* 0000 6" MoDOT TYPE 1 CRUSHED STONE BASE COMPACTED SOIL SUBGRADE

NOTES:

- CURB & GUTTER SHALL BE PORTLAND CEMENT CONCRETE.
 EXPANSION JOINTS AND CONTRACTION JOINTS IN CURB & GUTTER SHALL COINCIDE WITH STREET JOINTS IN CONCRETE STREETS AND SHALL BE INSTALLED AT DISTANCES NO GREATER THAN 10' IN CURB & GUTTER ALONG ASPHALT STREETS.
 WHERE EXISTING PAVEMENT IS CONCRETE, CURB REPLACEMENT SHALL BE DOWELED INTO THE STREET WITH 12" LONG #4 BARS AT 24" CTR



TYPE 1 CURB (TYPICAL)

2.401

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US HWY 50 (BROADWAY BLVD.) PUBLIC IMPROVEMENTS

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HWY 50 (BROADWAY BLVD.) PUBLIC IMPROVEMENTS

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ALL AREAS OF THE PEDESTRIAN ACCESS ROUTE MUST BE COMPLIANT WITH THE AMERICANS WITH DISABILITIES ACT - GUIDELINES FOR

POND WATER.

GENERAL NOTES:

ACCESSIBLE PUBLIC RIGHTS OF WAY". EXCEPTIONS MUST BE APPROVED BY THE ENGINEER. ALL OTHER AREAS OF NON-COMPLIANCE SHALL BE REMOVED AND CORRECTED AT THE CONTRACTOR'S EXPENSE. THE SURFACES OF PEDESTRIAN ACCESS ROUTES AND ELEMENTS, AND SPACES REQUIRED TO CONNECT TO PEDESTRIAN ACCESS ROUTES, SHALL BE FIRM, STABLE, SLIP RESISTANT, AND SHALL NOT

SIDEWALK, RAMP AND LANDING CROSS SLOPES SHALL BE 1.00% TO FACILITATE DRAINAGE (2.00% MAX.).

THE CROSS SLOPE OF THE CONTINUOUS PEDESTRIAN ACCESS ROUTE THROUGH ENTRANCES, ALLEYS, AND SIDE ROAD CONNECTIONS WITH STOP OR YIELD CONTROL SHALL BE 1.00% TO FACILITATE DRAINAGE (2.00% MAX.).

WHERE PEDESTRIAN ACCESS ROUTES ARE CONTAINED WITHIN PEDESTRIAN STREET CROSSINGS WITHOUT YIELD OR STOP CONTROL, THE CROSS SLOPE OF THE PEDESTRIAN ACCESS ROUTE SHALL BE 5.00% MAXIMUM.

WHERE PEDESTRIAN ACCESS ROUTES ARE CONTAINED WITHIN MIDBLOCK PEDESTRIAN STREET CROSSINGS, THE CROSS SLOPE OF THE PEDESTRIAN ACCESS ROUTE SHALL BE PERMITTED TO EQUAL THE STREET OR HIGHWAY GRADE.

30"x 48" CLEAR SPACE SHALL BE PROVIDED CENTERED ON THE PEDESTRIAN PUSH BUTTON.

BEYOND THE BOTTOM GRADE BREAK OF A CURB RAMP, A CLEAR SPACE 4'MINIMUM BY 4'MINIMUM SHALL BE PROVIDED WITHIN THE WIDTH OF THE PEDESTRIAN STREET CROSSING AND WHOLLY OUTSIDE THE PARALLEL VEHICLE TRAVEL LANE.

SIDE FLARES OF CURB RAMPS, IN THE PATH OF PEDESTRIAN TRAVEL (TRAVERSABLE), SHALL NOT EXCEED A SLOPE OF 1V:10H. SIDE FLARES OUTSIDE THE PEDESTRIAN PATH (NONTRAVERSABLE) MAY BE VERTICAL.

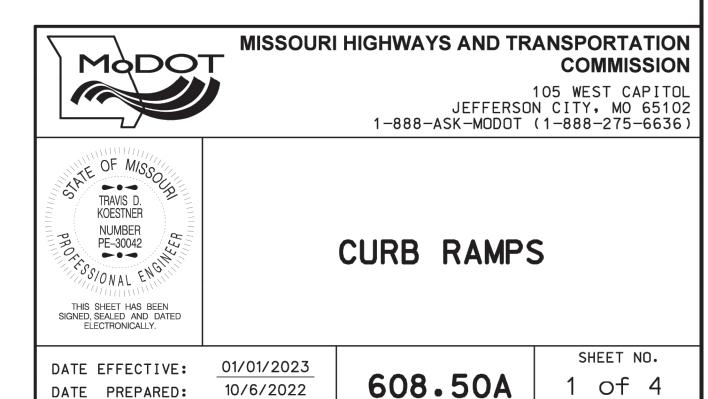
TRANSITION FROM SIDEWALK OR CURB RAMP TO GUTTER TO ROADWAY SHALL BE FLUSH.

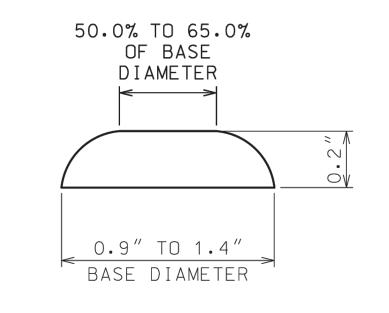
DETECTABLE WARNING SURFACES (TRUNCATED DOMES) SHALL BE PREFORMED AND INSTALLED AS PER MANUFACTURER'S RECOMMENDATIONS. STAMPED CONCRETE WILL NOT BE ACCEPTED.

THE DETECTABLE WARNING SURFACE SHALL CONTRAST VISUALLY WITH ADJOINING SURFACES, EITHER LIGHT-ON-DARK OR DARK-ON-LIGHT. TRUNCATED DOMES SHALL SPAN THE FULL WIDTH OF THE RAMP OR LANDING 24" DEEP.

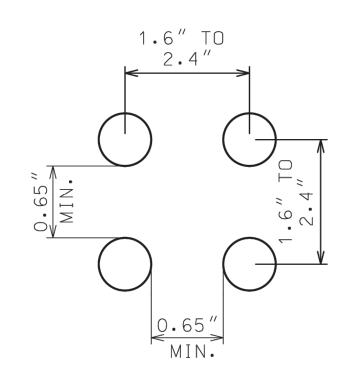
DETECTABLE WARNING SURFACES SHALL BE ALIGNED PERPENDICULAR OR RADIAL TO THE BREAK BETWEEN THE RAMP, LANDING OR BLENDED TRANSITION, AND THE STREET.

WHERE THE BOTTOM GRADE BREAK OF A CURB RAMP IS LESS THAN 5' FROM THE BACK OF CURB, DETECTABLE WARNINGS SHALL BE LOCATED ON THE RAMP SURFACE AT THE BACK OF THE CURB. WHERE THE GRADE BREAK IS GREATER THAN 5' FROM THE BACK OF CURB, THE DETECTABLE WARNING SHALL BE LOCATED ON THE LOWER LANDING.

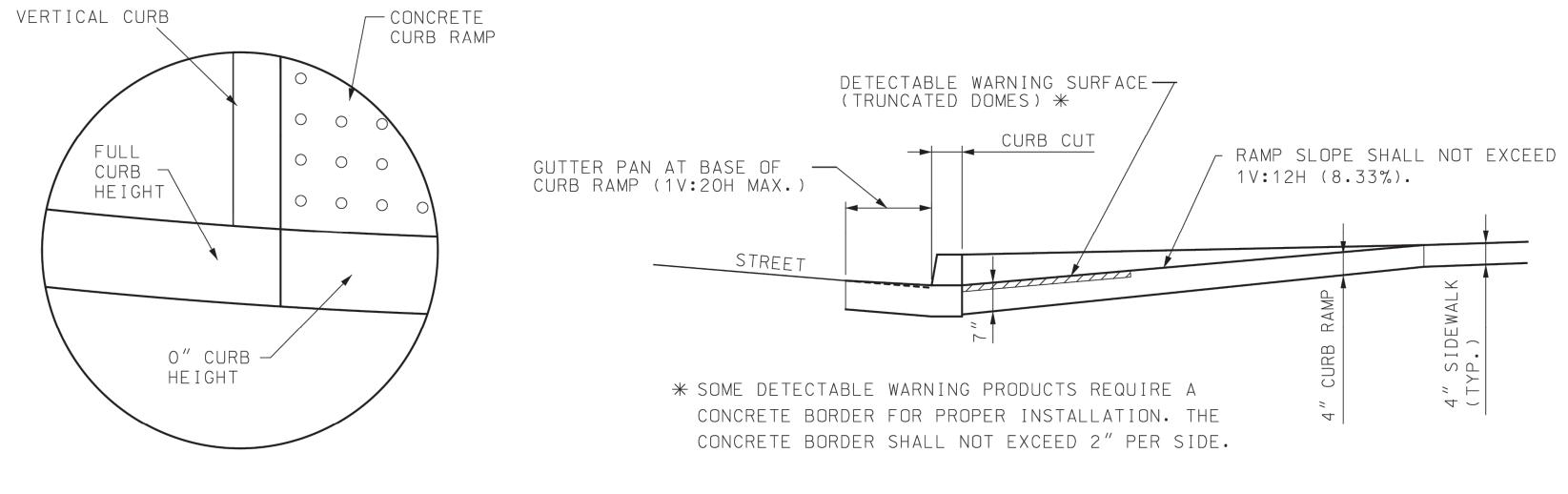




TRUNCATED DOMES CROSS SECTION



TRUNCATED DOMES SPACING



 $A \longrightarrow$

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• • • • • • • • •

SAME AS

APPROACH SIDEWALK (5' MIN.)

CURB RAMP DETAIL

DETAIL A

0 0 0 0 0 0 0 0000000

VERTICAL

CURB

DETAIL A

TRAVERSABLE FLARE

CURB HEIGHT

TRANSITION

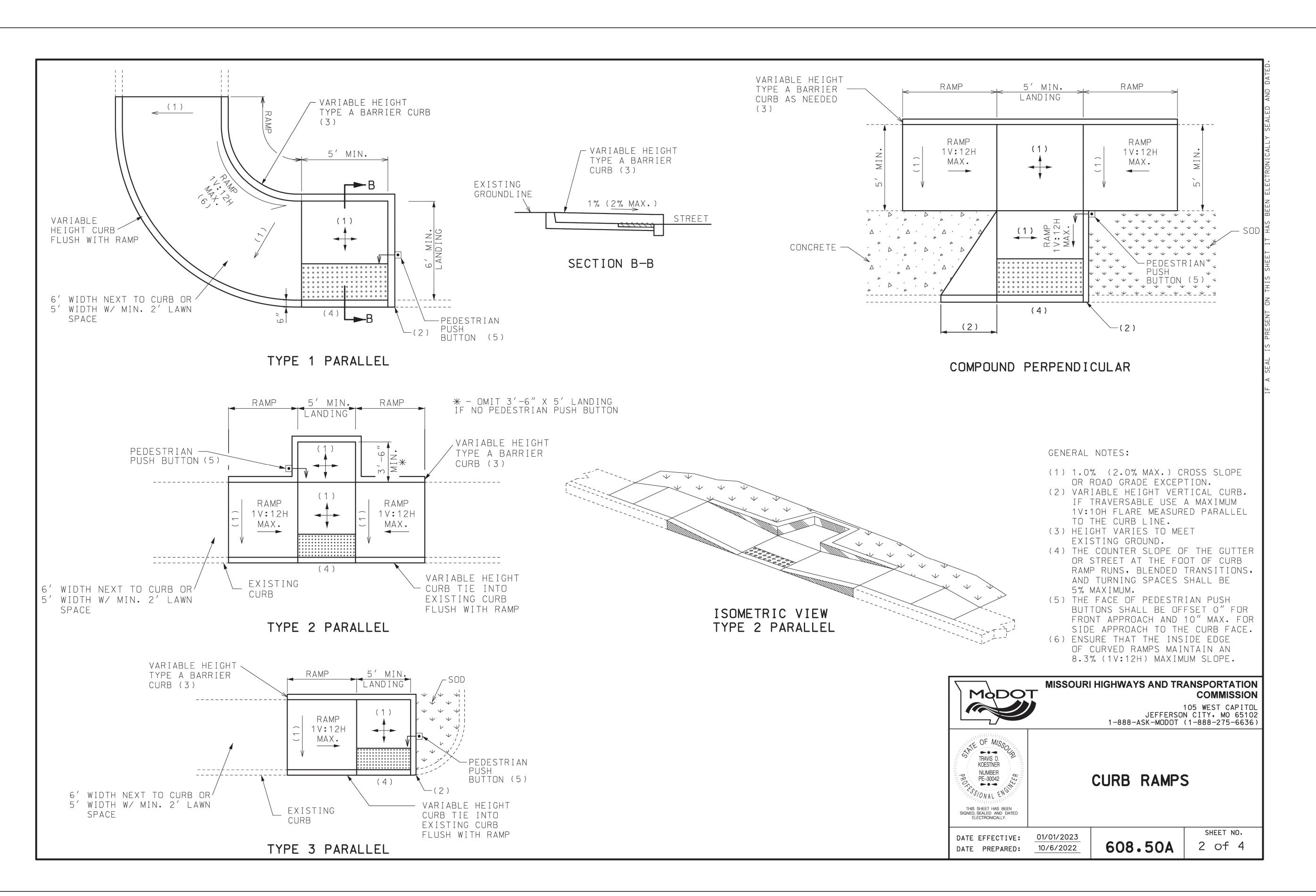
TRUNCATED DOME

PATTERN 24" DEEP

1V;1OH MAX.

SECTION A-A

SHEET



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EV. DATE DESCRIPTION

US HWY 50 (BROADWAY BLVD.) PUBLIC IMPROVEMENTS

US SEDALIA, MISSOUI

 drawn by:
 DAC

 designed by:
 DAC

 project no.:
 025-01245

 date:
 07.25.2025

SHEET 14 of 33

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US HWY 50 (BROADWAY BLVD.) PUBLIC IMPROVEMENTS

J SEDALIA, MISSO

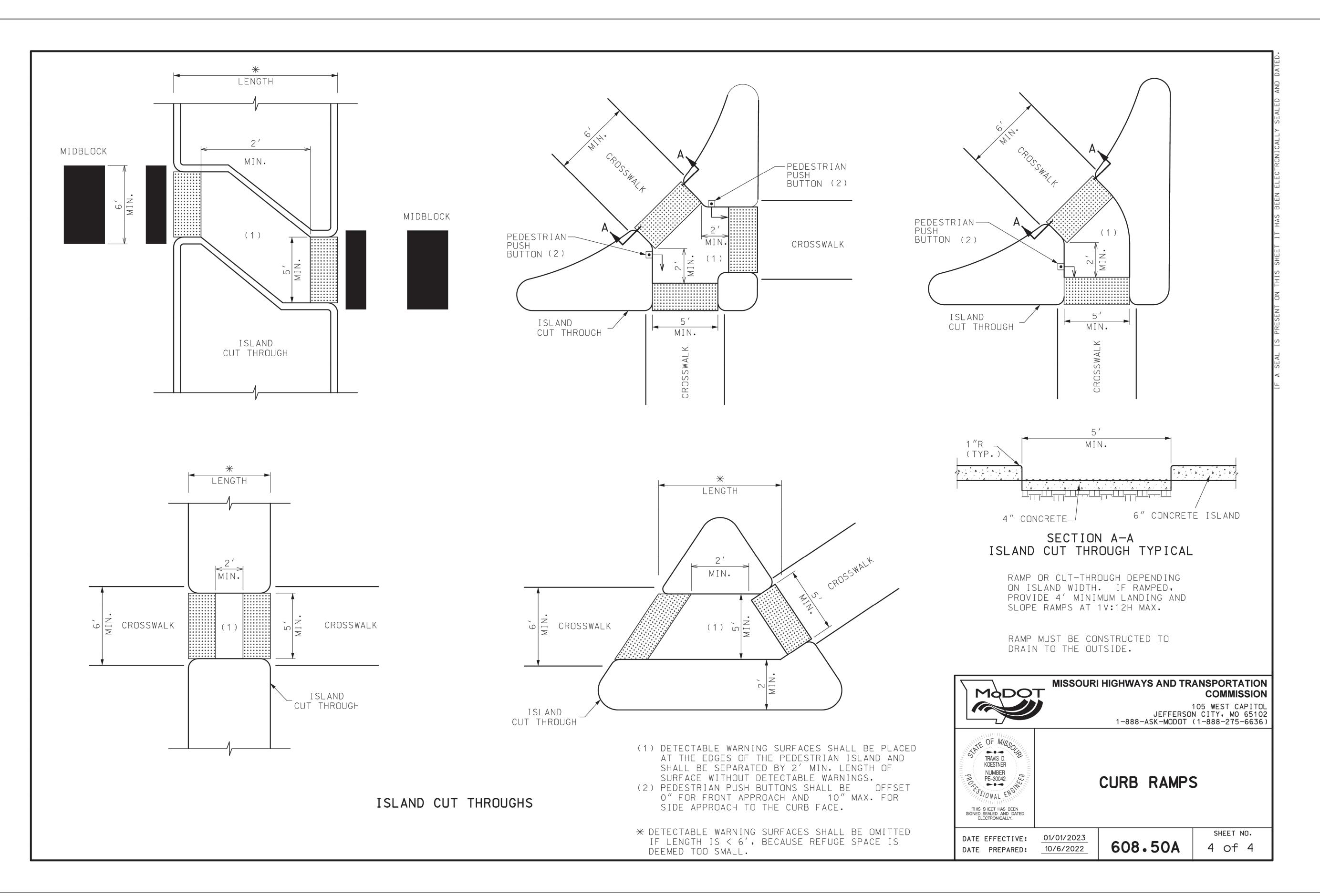
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 DAC

 designed by:
 DAC

 project no.:
 025-01245

 date:
 07.25.2025

SHEET 15 of 33





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DATE					
REV. NO.					

STANDARD DETAILS

US HWY 50 (BROADWAY BLVD.) PUBLIC IMPROVEMENTS

DAC DAC 025-01245 07.25.2025 designed by:_ project no.:__

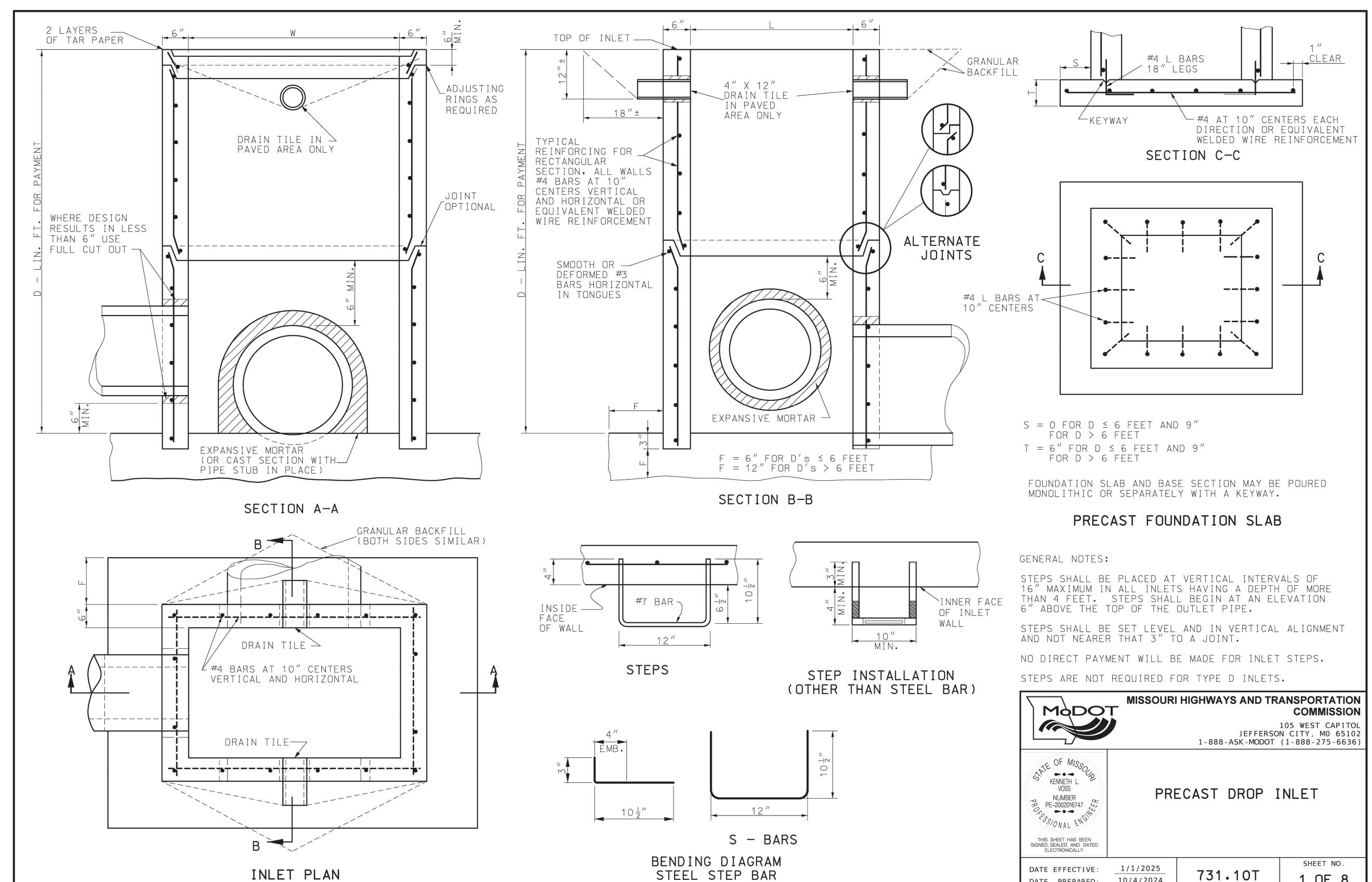
> SHEET 16 of 33

1 OF 8

10/4/2024

DATE PREPARED:

SHEET 17 of 33





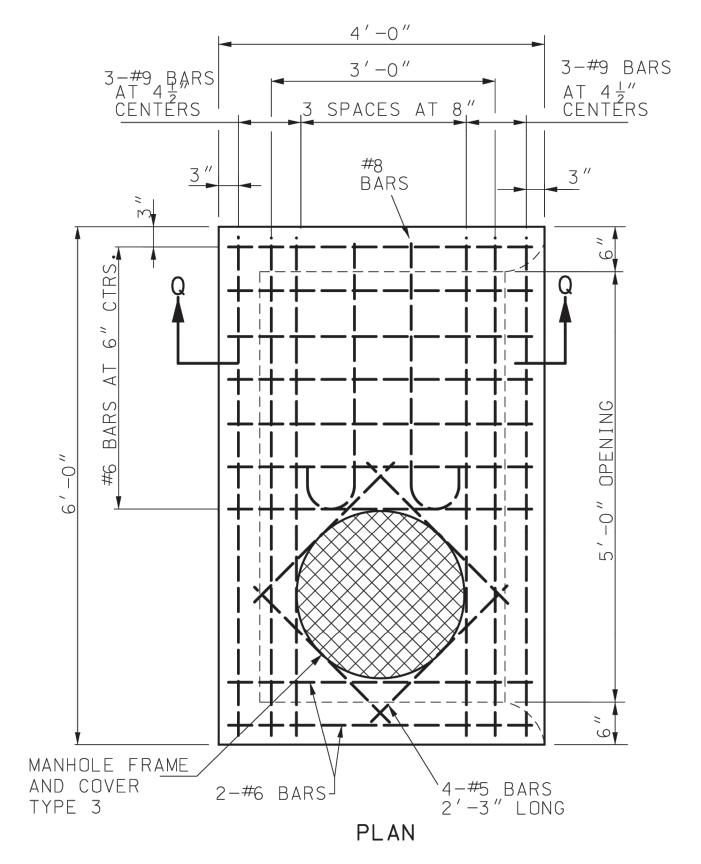
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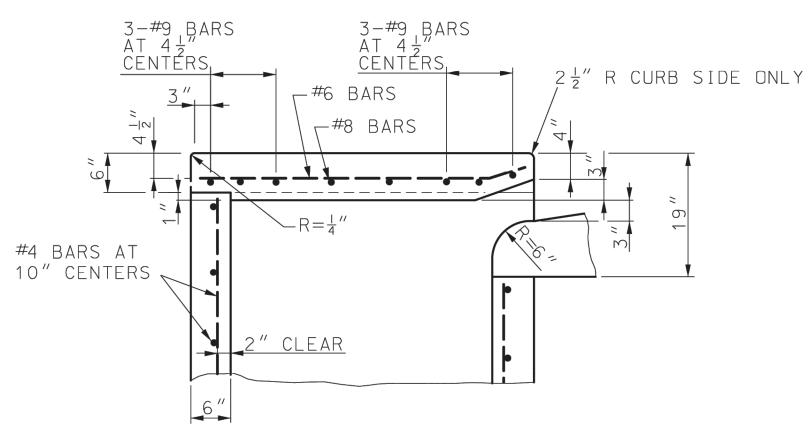
US HWY 50 (BROADWAY BLVD.) PUBLIC IMPROVEMENTS

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> SHEET 18 of 33

LID FOR ADJACENT SECTIONS





SECTION Q-Q

OPTIONAL PRECAST CURB INLET 5'-0" OPENING

> OTHER DETAILS ARE SAME AS FOR THE 2'-6" OPENING DROP INLET THIS SHEET.

GENERAL NOTES:

NOTES PERTAINING TO TYPE T:

THE LENGTH AND DEPTH OF THE INLET SHALL BE AS SHOWN ON THE PLANS.

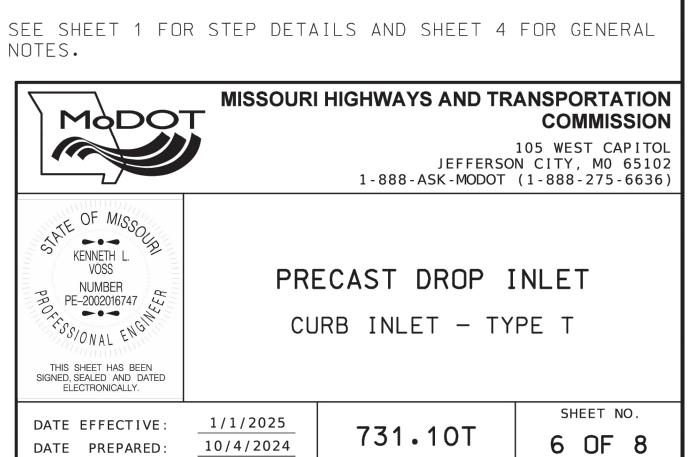
WALLS BETWEEN THE ADJACENT SECTIONS SHALL BE SEALED IN ACCORDANCE WITH SECTION 726.3.1 OF THE STANDARD SPECIFICATIONS.

IF DEPTH OF INLET EXCEEDS 6 FEET THE PRECAST UNITS MAY BE FURNISHED IN TWO OR MORE SECTIONS.

IF TWO OR MORE SECTIONS ARE USED, THE TYPE 3 MANHOLE FRAME AND COVER SHALL BE IN THE DOWNSTREAM SECTION ONLY.

IF A 5 FOOT OPENING IS REQUIRED, TWO 2'-6" OPENING SECTIONS OR ONE 5 FOOT OPENING SECTION MAY BE PROVIDED AT THE CONTRACTOR'S OPTION.

SEE SHEET 1 FOR STEP DETAILS AND SHEET 4 FOR GENERAL



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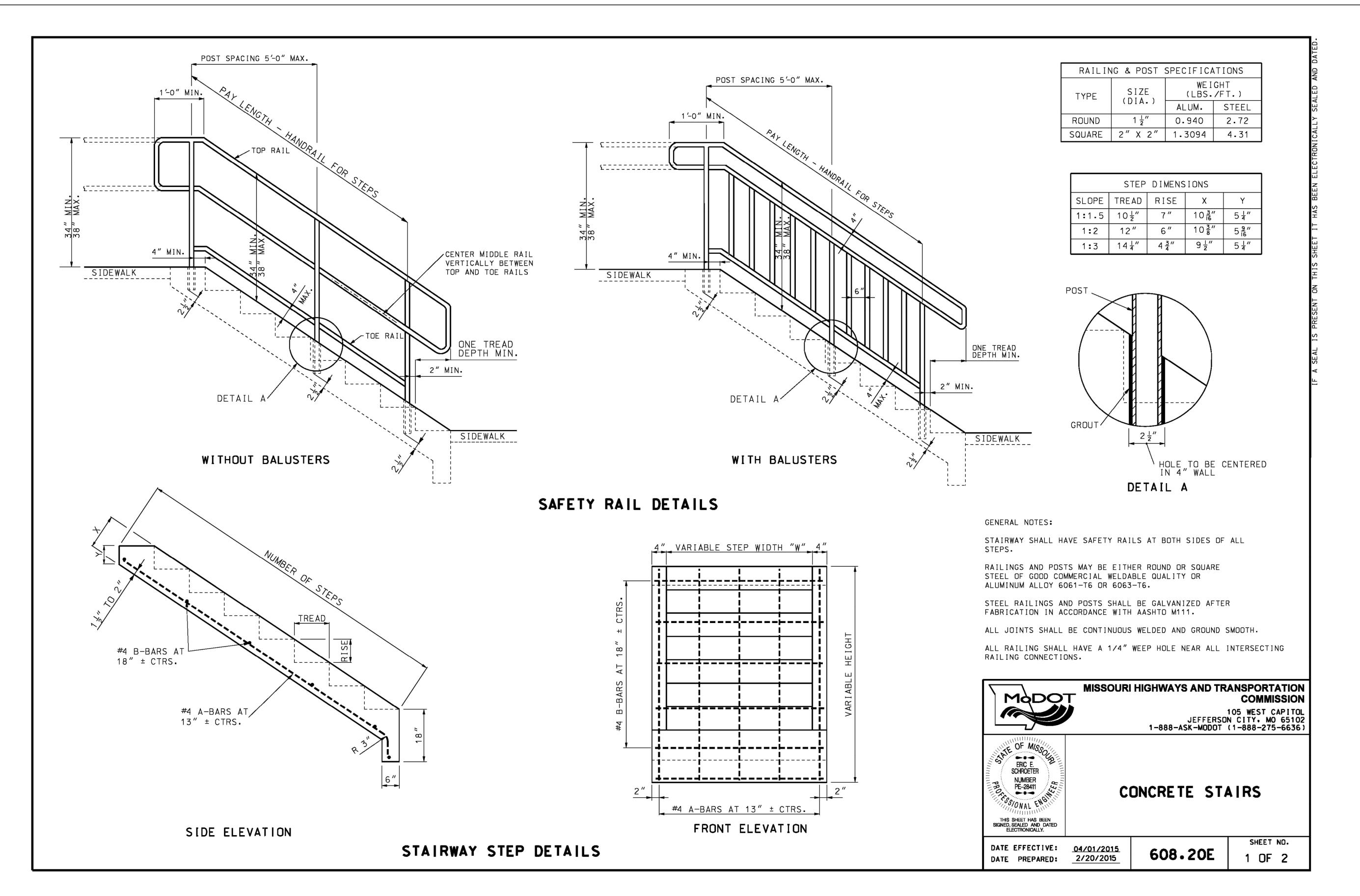
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US HWY 50 (BROADWAY BLVD.) PUBLIC IMPROVEMENTS

STANDARD

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> SHEET 19 of 33





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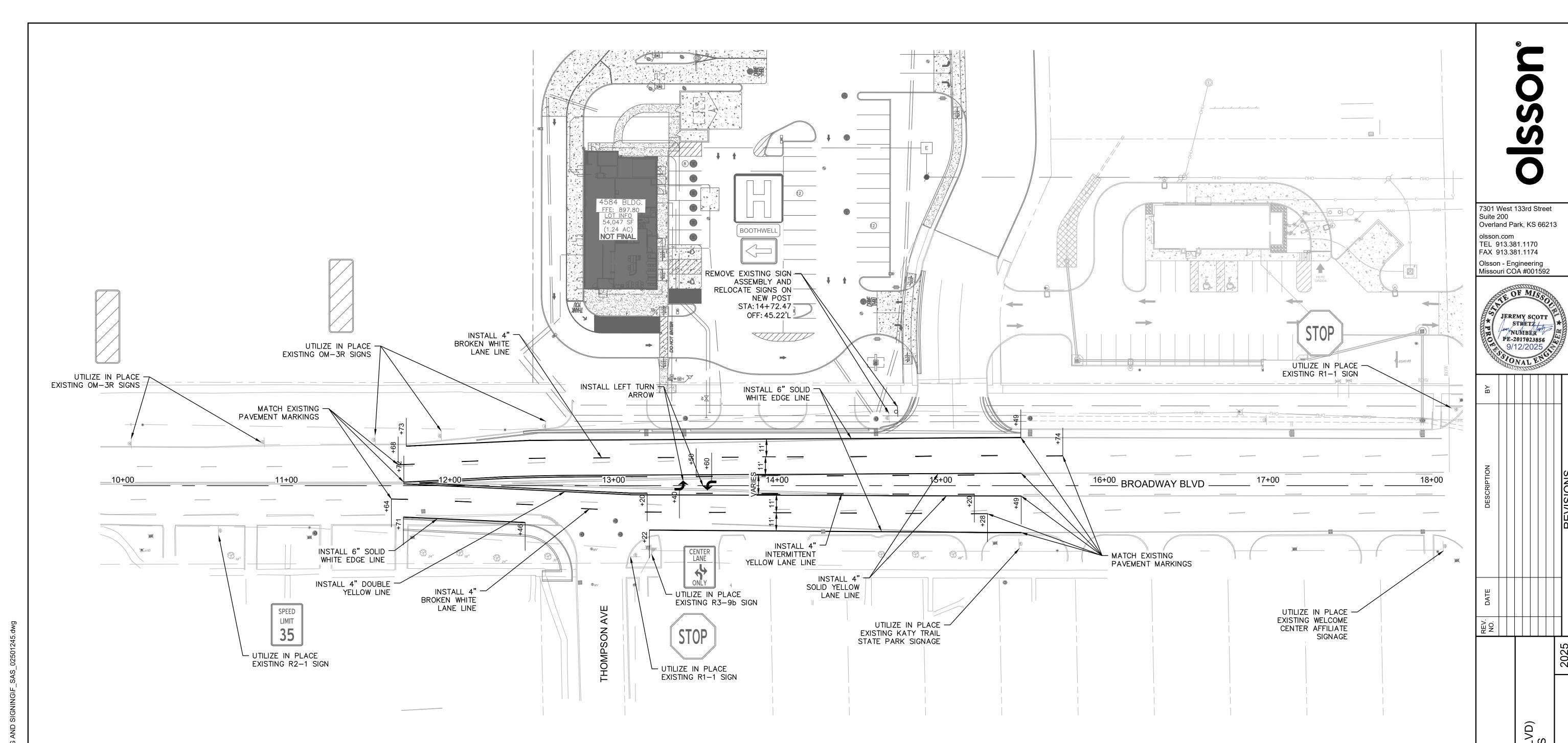
US HWY 50 (BROADWAY BLVD.) PUBLIC IMPROVEMENTS

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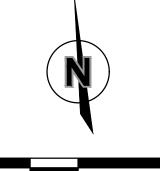
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GENERAL NOTES:

- 1. SEE STANDARD PLAN 620.00 FOR PAVEMENT MARKING DETAILS. ALL NEW LONGITUDINAL PAVEMENT MARKINGS SHALL BE CLASS 1 HIGH BUILD PAINT WITH TYPE P BEADS. ALL TURN ARROWS SHALL BE PREFORMED THERMOPLASTIC AND INSTALLED VIA THERMAL INLAY.
- 2. CONTRACTOR TO WATER-BLAST CONFLICTING PAVEMENT MARKINGS.
- 3. INTERMITTENT WHITE AND YELLOW MARKINGS ARE 10' LONG WITH 30' SKIPS.



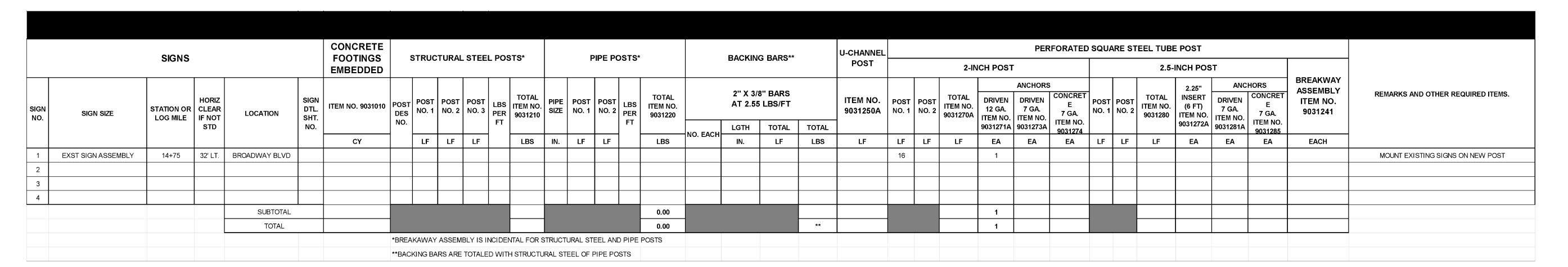
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US HWY 50 (BROADWAY BLVD) PUBLIC IMPROVEMENTS

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PAVEMENT MARKING AND SIGNING PLAN



					PAVEME	NT MARKING		
STA.	STA.	LOCATION		HIGH BUILD WAT	TERBORNE PAINT		PREFORMED THERMOPLASTIC	REMARKS
			4" BROKEN	4" SOLID	6" SOLID	4" BROKEN	LEFT-TURN	
			LANE LINE	LANE LINE	EDGE LINE	LANE LINE	ARROW	
			YELLOW	YELLOW	WHITE	WHITE	WHITE	
			(L.F.)	(L.F.)	(L.F.)	(L.F.)	(EACH)	
1+68	15+74	BROADWAY BLVD				406		
11+72	13+21	BROADWAY BLVD		149				
11+72	13+21	BROADWAY BLVD		149				
11+72	15+50	BROADWAY BLVD		378				
11+72	15+50	BROADWAY BLVD		378				
11+73	15+49	BROADWAY BLVD			376			
13+55	15+25	BROADWAY BLVD	170					
13+55	15+25	BROADWAY BLVD	170					
11+64	15+28	BROADWAY BLVD				364		
11+72	12+47	BROADWAY BLVD			75			
13+20	15+48	BROADWAY BLVD			228			
13+46		BROADWAY BLVD					1	
13+64		BROADWAY BLVD					1	
0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0								
		Subtotal:	340	1054	679	770	2	
		Multipier:	1.00	1.00	1.00	0.25	1.00	
		TOTAL:	340	1054	679	193	2	

Pay Item	Item Description	Quantity
6206000D	4" White	193
6205901B	4" Yellow	1394
6205902B	6" Solid White	679
6200021	Left/Right Arrow	2



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US HWY 50 (BROADWAY BLVD) PUBLIC IMPROVEMENTS PAVEMENT MARKING AND SIGNING PLAN

drawn by: ___ designed by: __ project no.:___

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LEGEND

Phase 1 Work Area

Phase 2 Work Area

Phase 3 Work Area

CONSTRUCTION SEQUENCE:

PHASE 1.) PARTIAL CLOSURE OF SOUTHBOUND LANE ALONG HANCOCK AVENUE. REMAINING PORTION OF UTILITY CONNECTION TO BE CONSTRUCTED DURING THIS PHASE. ACCESSES TO BE MAINTAINED FOR EXISTING TACO BELL SITE. VEHICULAR DETOUR TO BE PROVIDED ALONG MARVIN AVENUE. CONTRACTOR TO MAINTAIN SIDEWALK ACCESS THROUGH CONSTRUCTION PHASE FOR PEDESTRIANS.

PHASE 2.) CLOSURE OF NORTHERNMOST WESTBOUND LANE ALONG BROADWAY BLVD. REMAINING UTILITY CONNECTION AND NEW CURB LINE TO BE CONSTRUCTED DURING THIS PHASE. ACCESS TO EXISTING TACO BELL TO BE MAINTAINED OFF OF HANCOCK AVENUE. CONTRACTOR TO MAINTAIN SIDEWALK ACCESS THROUGH CONSTRUCTION PHASE FOR PEDESTRIANS.

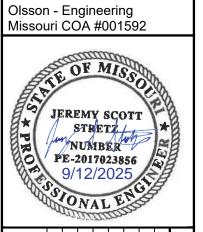
PHASE 3.) CLOSURE OF SOUTHERNMOST EASTBOUND LANE ALONG BROADWAY BLVD. NEW CURB LINE AND SIDEWALK TO BE CONSTRUCTED DURING THIS PHASE. CONTRACTOR TO PROVIDE PEDESTRIAN DETOUR ROUTE AS SHOWN IN THE PLANS.

PHASE 4.) MOBILE OPERATION. CONTRACTOR TO INSTALL PERMANENT PAVEMENT MARKINGS DURING THIS PHASE. CONTRACTOR TO MAINTAIN ACCESS ALONG BROADWAY BLVD DURING THIS PHASE.



o' 15' 30' SCALE IN FEET Olsson

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TRAFFIC CONTROL PLAN
CONSTRUCTION SEQUENCING
US HWY 50 (BROADWAY BLVD)
PUBLIC IMPROVEMENTS

SEDALIA, N

drawn by: SEL designed by: JMP project no.: 025-01245 date: 07/25/2025

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SHEET 24 of 33

0' 25' 50' SCALE IN FEET





LEGEND

Phase 2 Work Area

Ш Туре III Barricade

o Sign Symbol

Channelizing Device

⇒ Direction of Traffic

Flashing Arrow Panel

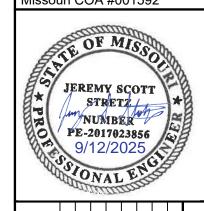
NOTES:

- ALL TRAFFIC CONTROL SHALL FOLLOW CURRENT MODOT/MUTCD STANDARDS AND SPECIFICATIONS.
- 2) CONTRACTOR TO MAINTAIN SIDEWALK ACCESS ALONG THE NORTHSIDE OF BROADWAY BLVD DURING CONSTRUCTION.
- 3) LANE CLOSURE ELEMENTS SHOULD BE SUITABLE FOR 24-HOUR CONDITIONS AND MEET MODOT/MUTCD APPLICABLE CRITERIA FOR THOSE CONDITIONS, INCLUDING BUT NOT LIMITED TO SIGN RETRO REFLECTIVITY.



SCALE IN FEET

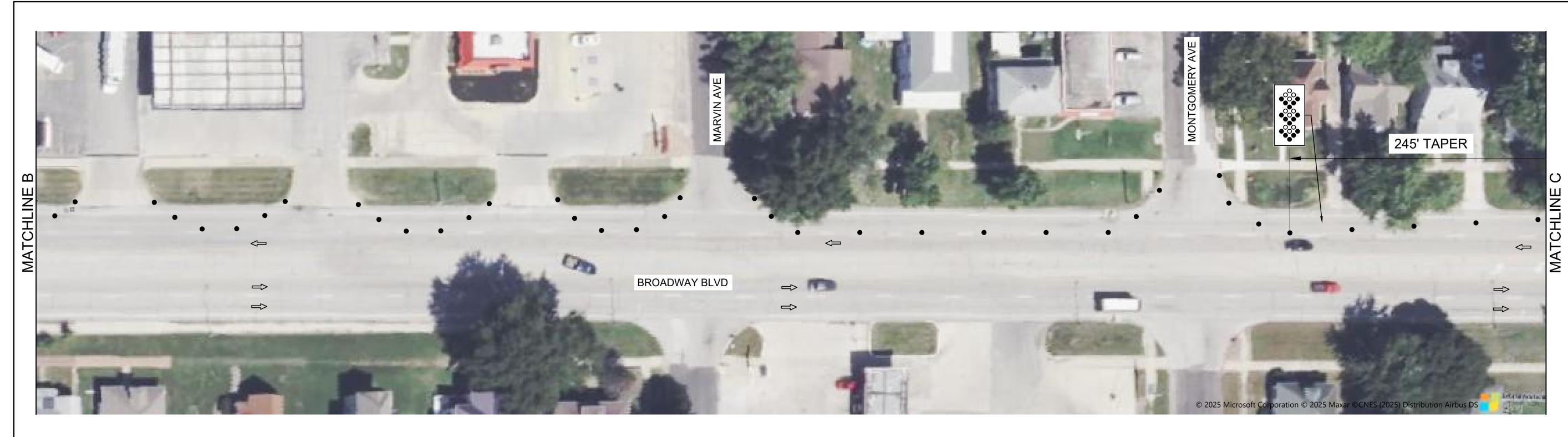
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US HWY 50 (BROADWAY BLVD) PUBLIC IMPROVEMENTS

designed by: JMP 025-01245 07/25/2025

SHEET 25 of 33





LEGEND

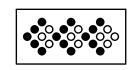
Phase 2 Work Area

Ш Туре III Barricade

→ Sign Symbol

Channelizing Device

⇒ Direction of Traffic



Flashing Arrow Panel

NOTES:

- 1) ALL TRAFFIC CONTROL SHALL FOLLOW CURRENT MODOT/MUTCD STANDARDS AND SPECIFICATIONS.
- 2) CONTRACTOR TO MAINTAIN SIDEWALK ACCESS ALONG THE NORTHSIDE OF BROADWAY BLVD DURING CONSTRUCTION.
- 3) LANE CLOSURE ELEMENTS SHOULD BE SUITABLE FOR 24-HOUR CONDITIONS AND MEET MODOT/MUTCD APPLICABLE CRITERIA FOR THOSE CONDITIONS, INCLUDING BUT NOT LIMITED TO SIGN RETRO REFLECTIVITY.



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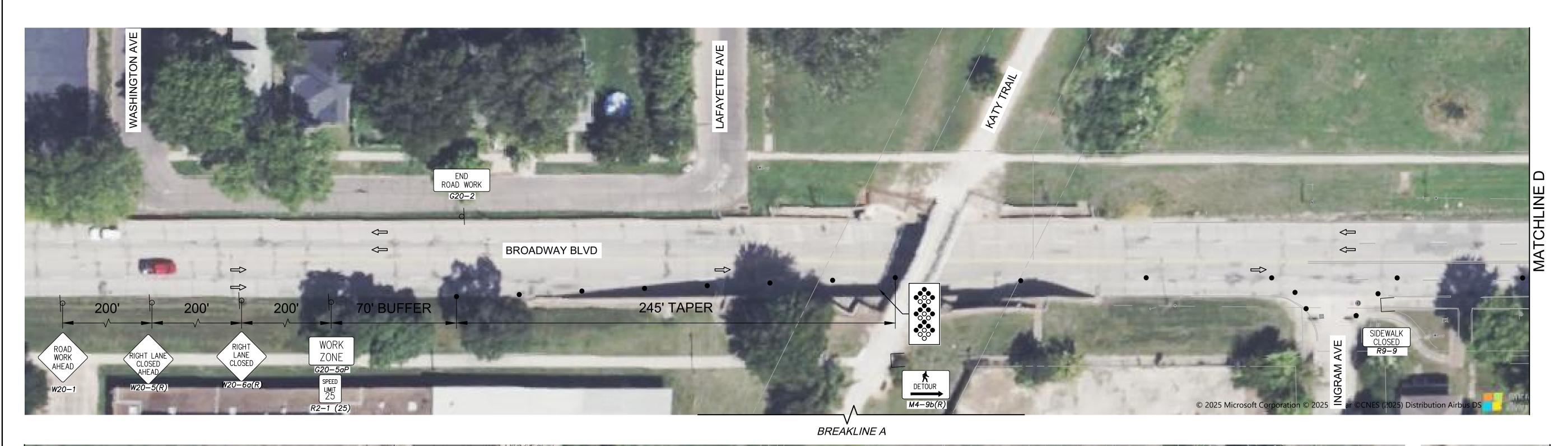
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US HWY 50 (BROADWAY BLVD) PUBLIC IMPROVEMENTS

JMP 025-01245 07/25/2025

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BREAKLINE A 10TH STREET

NOTES:

ALL TRAFFIC CONTROL SHALL FOLLOW CURRENT MODOT/MUTCD STANDARDS AND SPECIFICATIONS.

BREAKLINE B

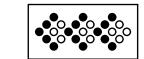
2) LANE CLOSURE ELEMENTS SHOULD BE SUITABLE FOR 24—HOUR CONDITIONS AND MEET MODOT/MUTCD APPLICABLE CRITERIA FOR THOSE CONDITIONS, INCLUDING BUT NOT LIMITED TO SIGN RETRO REFLECTIVITY.



LEGEND

Phase 2 Work Area

- Ш Туре III Barricade
- o Sign Symbol
- Channelizing Device
- ⇒ Direction of Traffic



Flashing Arrow Panel



SCALE IN FEET

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US HWY 50 (BROADWAY BLVD) PUBLIC IMPROVEMENTS

designed by: JMP 025-01245 07/25/2025

> SHEET 27 of 33



LEGEND

Phase 3 Work Area

Ш Туре III Barricade

o Sign Symbol

Channelizing Device

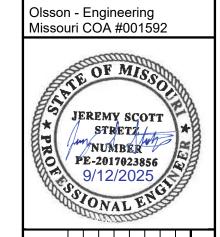
⇒ Direction of Traffic

NOTES:

- ALL TRAFFIC CONTROL SHALL FOLLOW CURRENT MODOT/MUTCD STANDARDS AND SPECIFICATIONS.
- 2) LANE CLOSURE ELEMENTS SHOULD BE SUITABLE FOR 24—HOUR CONDITIONS AND MEET MODOT/MUTCD APPLICABLE CRITERIA FOR THOSE CONDITIONS, INCLUDING BUT NOT LIMITED TO SIGN RETRO REFLECTIVITY.



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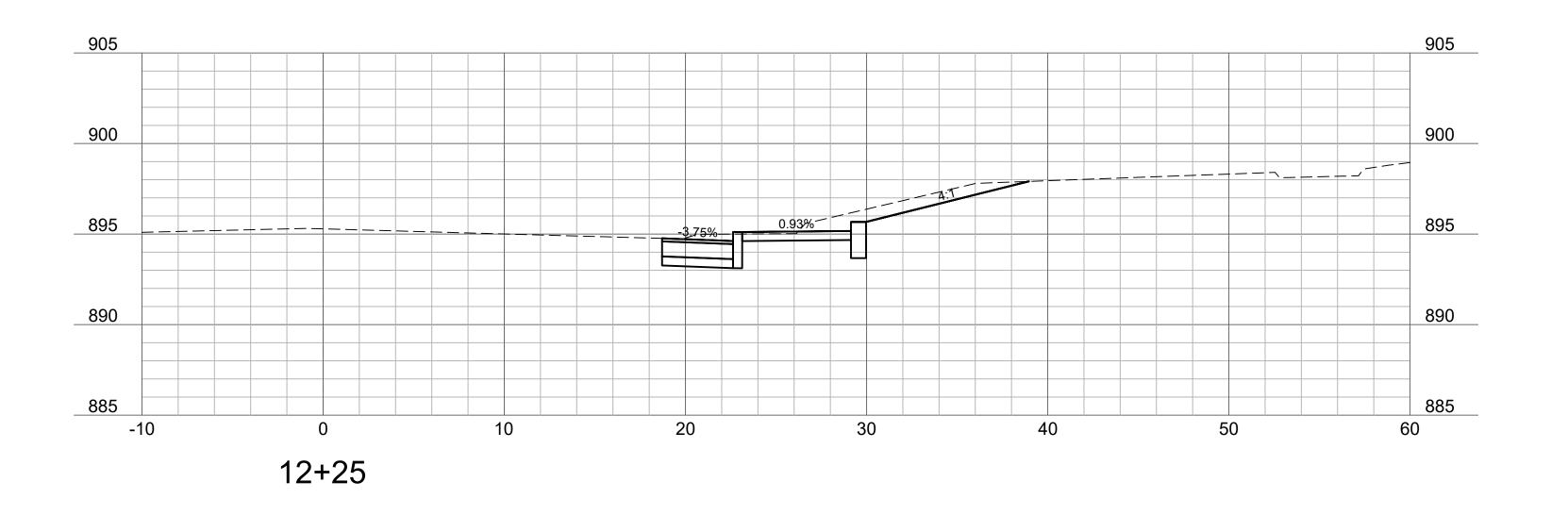


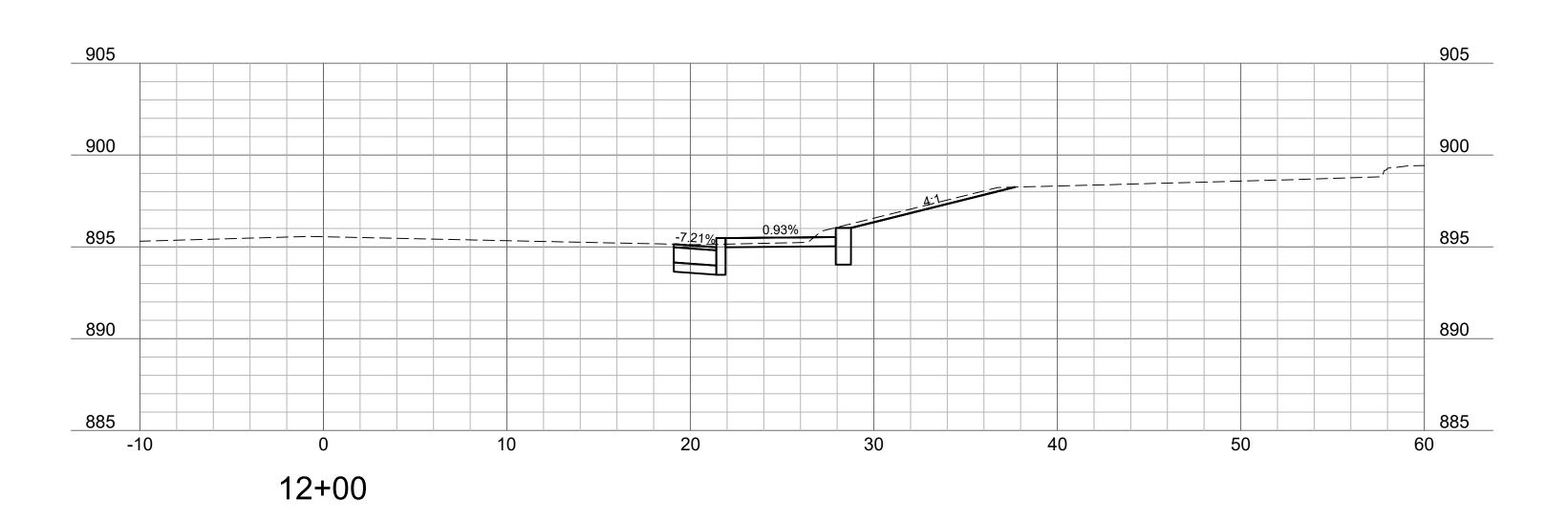
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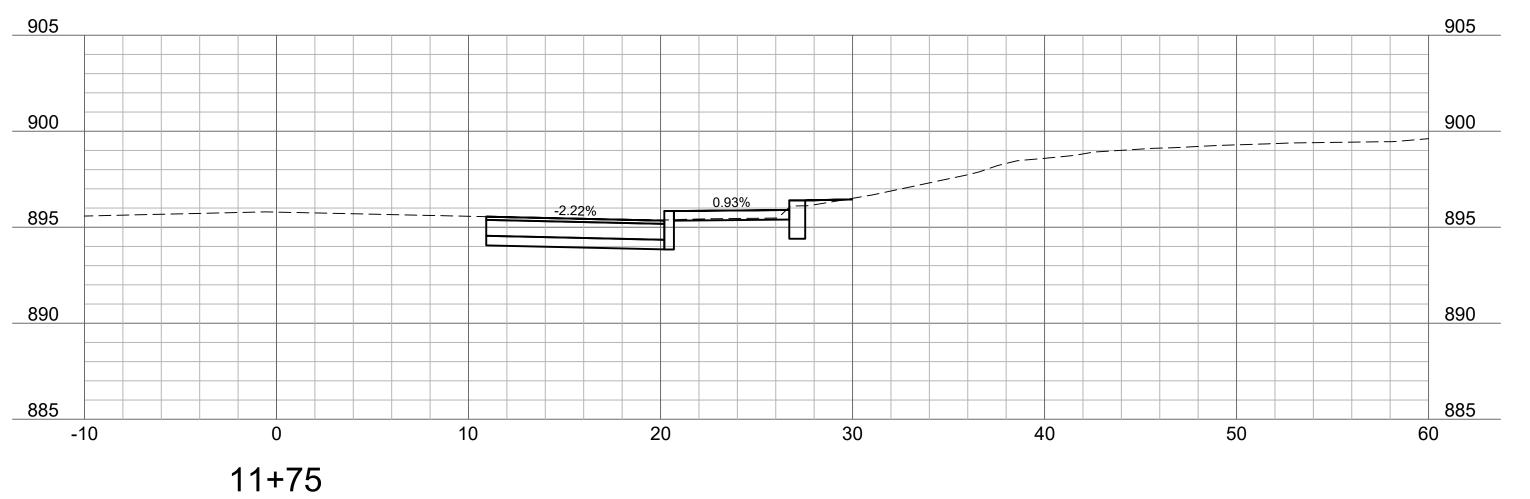
TRAFFIC CONTROL PLAN
PHASE 3
US HWY 50 (BROADWAY BLVD)
PUBLIC IMPROVEMENTS

drawn by: ______ SEL designed by: ______ JMP project no.: ______ 025-01245 date: ______ 07/25/2025 SHEET 28 of 33

0' 15' 30' SCALE IN FEET











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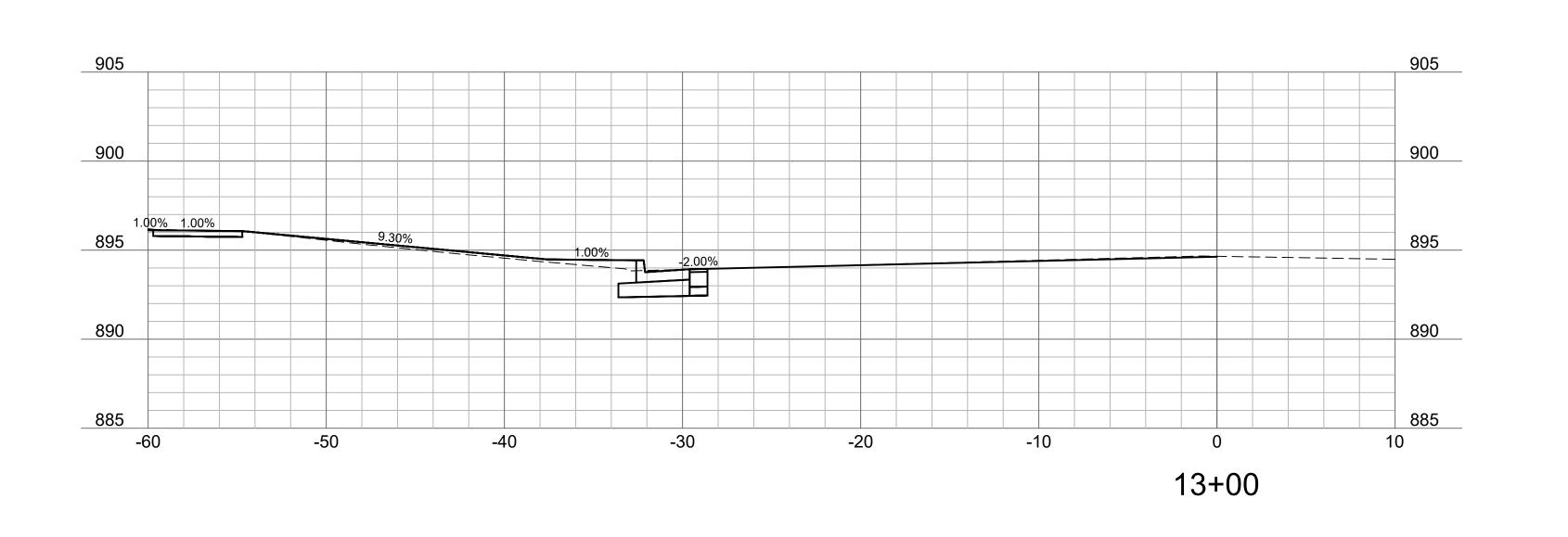
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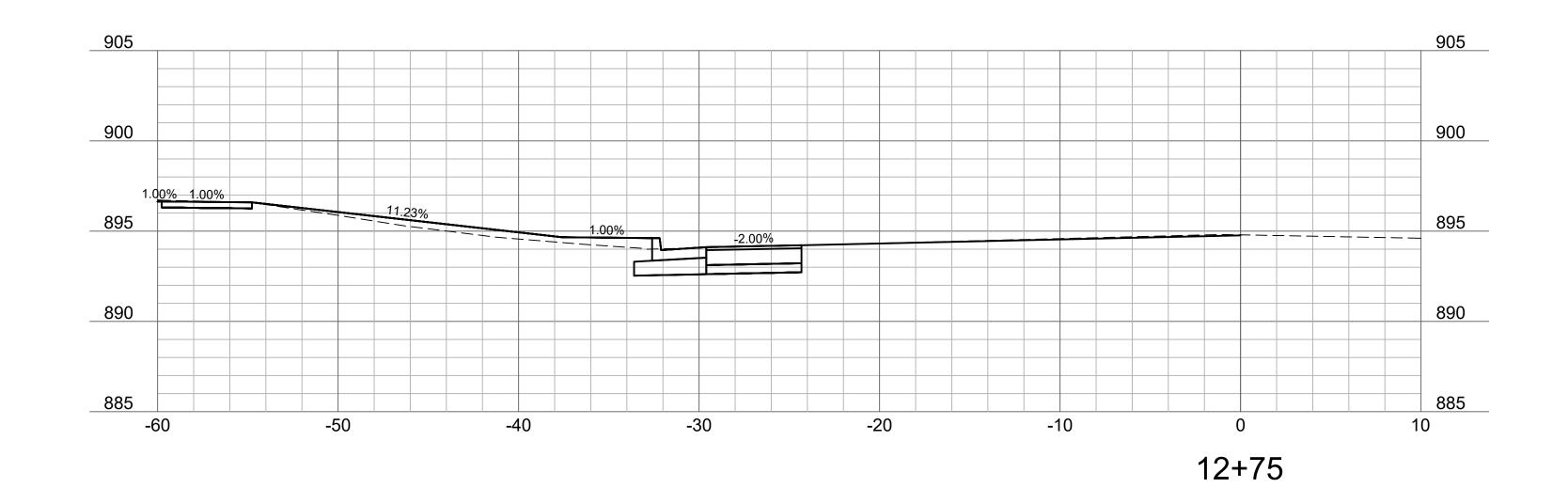
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US HWY 50 (BROADWAY BL) PUBLIC IMPROVEMENTS
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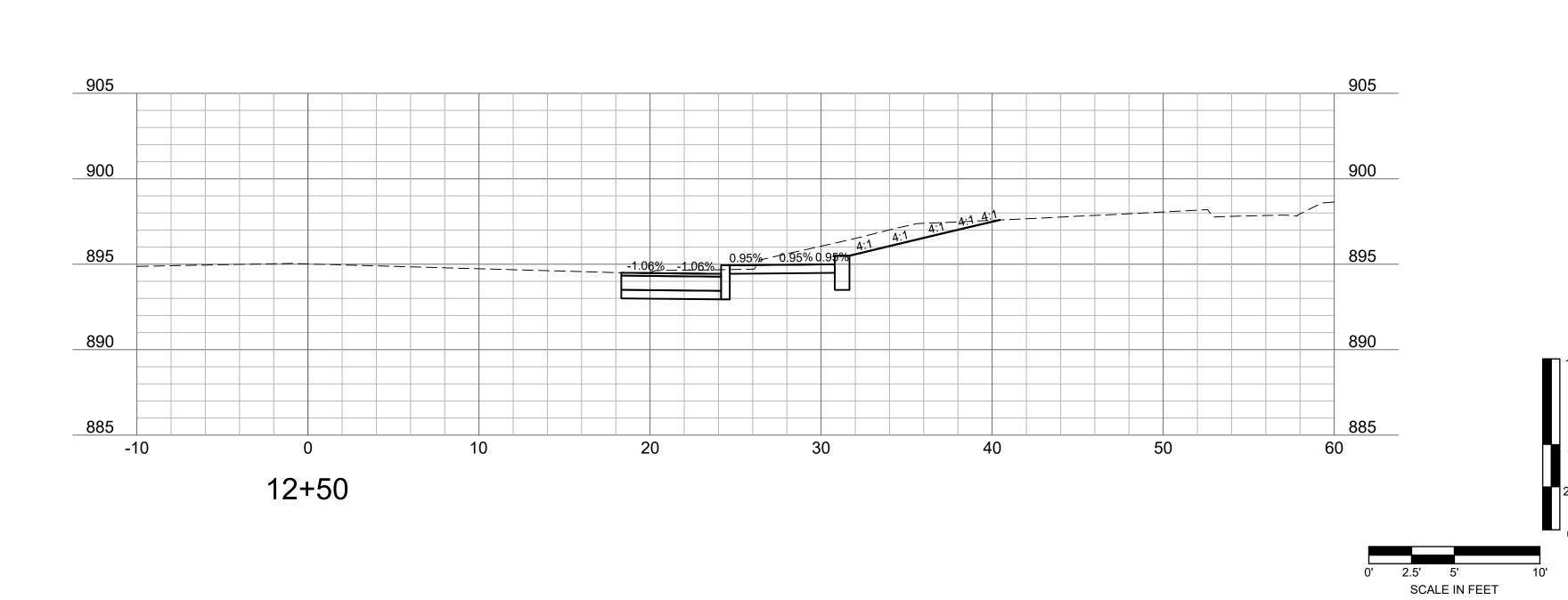
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0' 2.5' 5' SCALE IN FEET







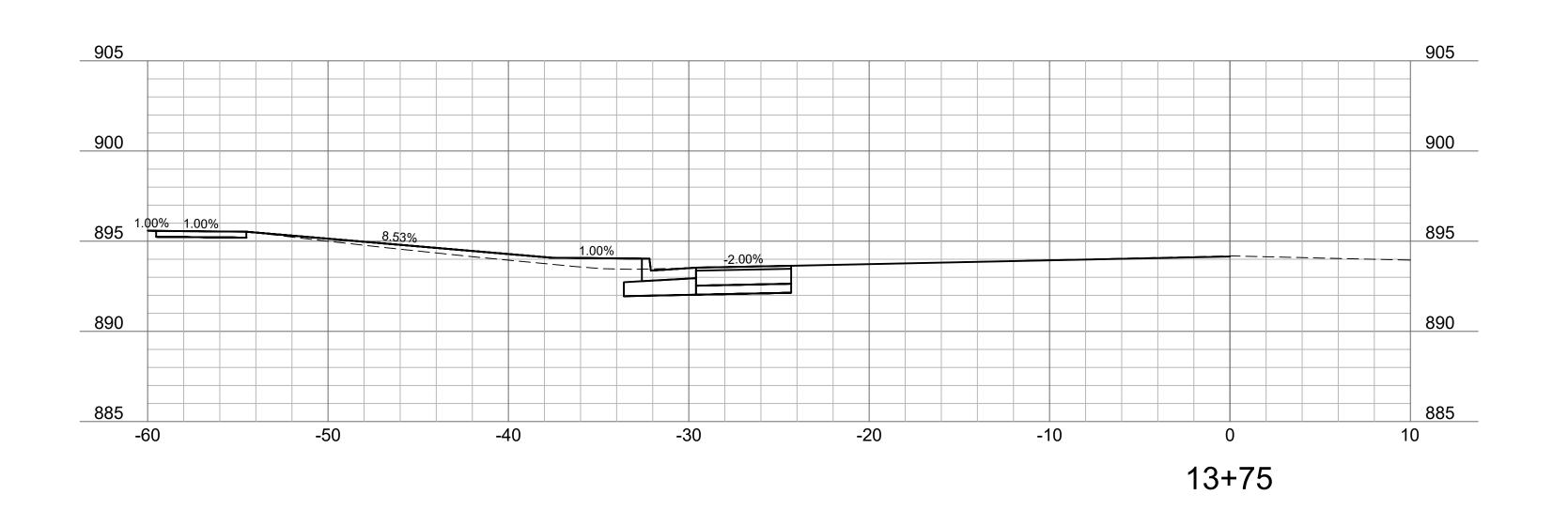


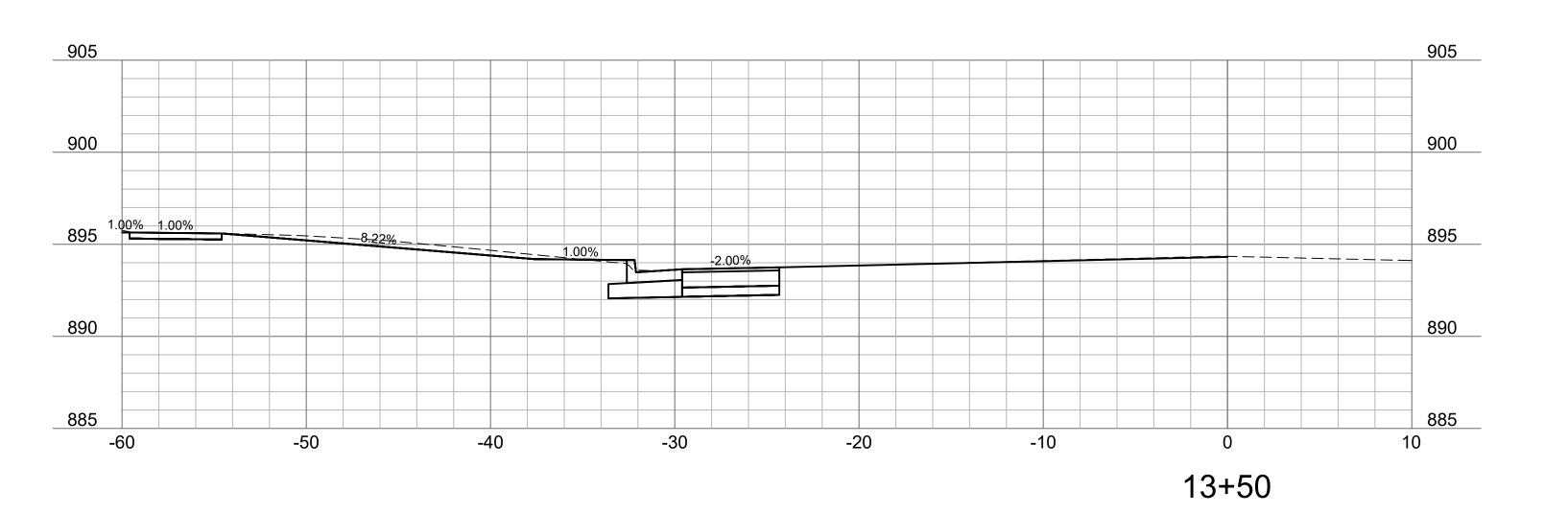
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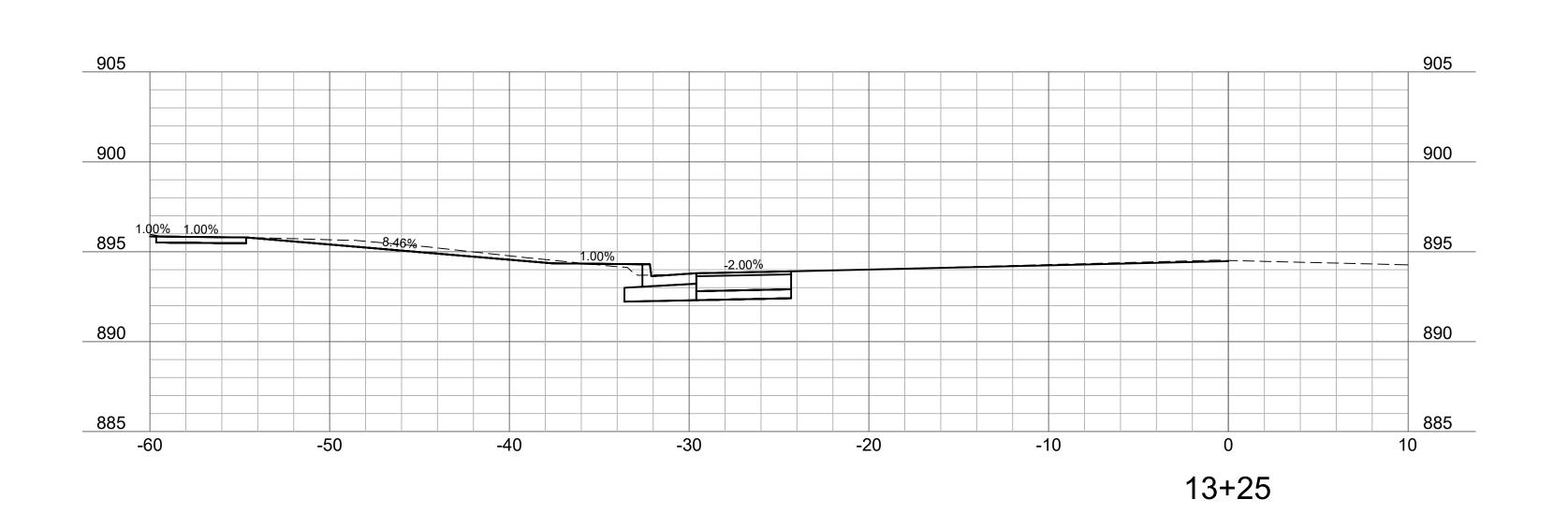
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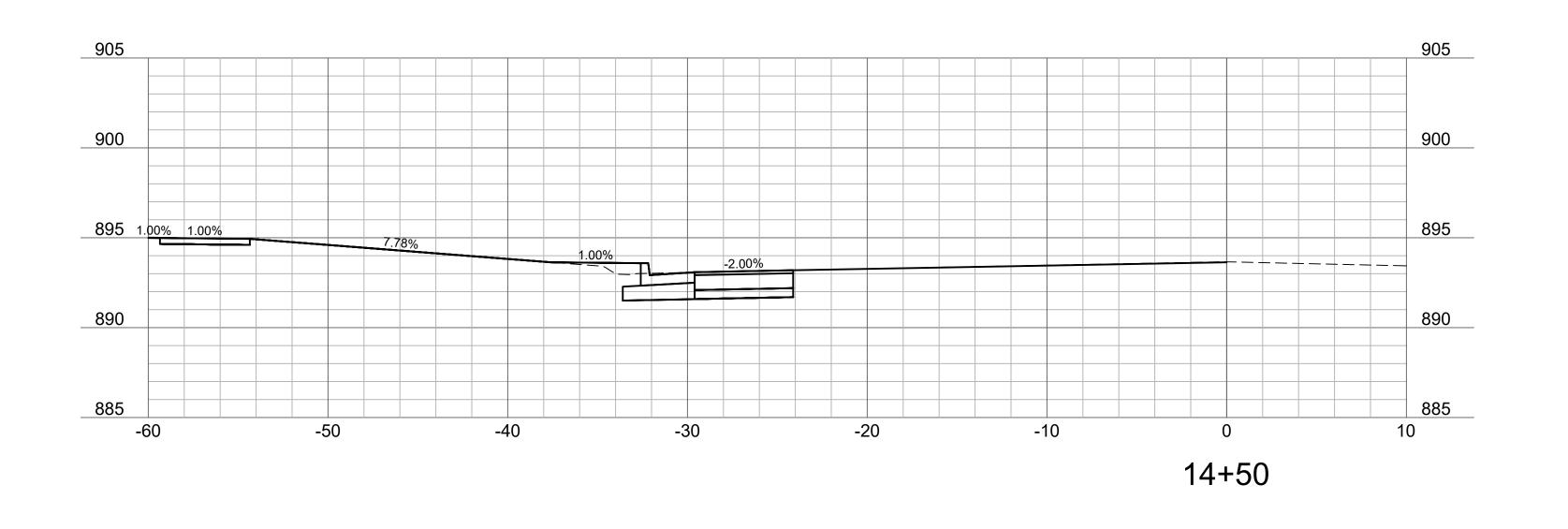
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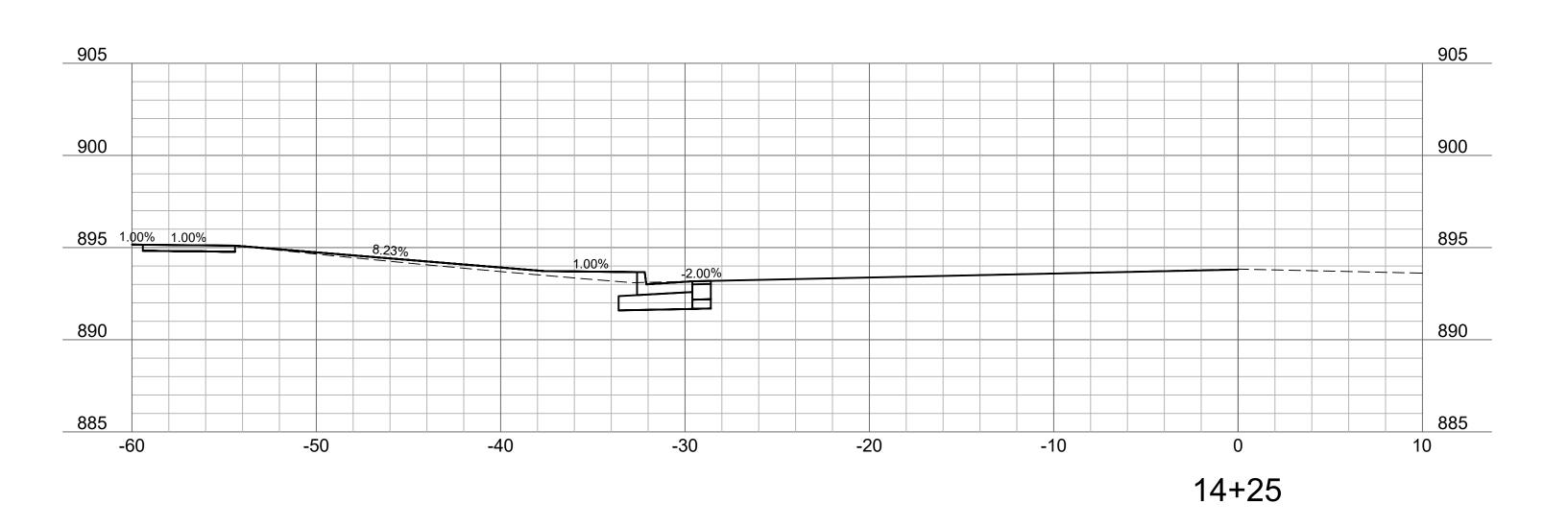
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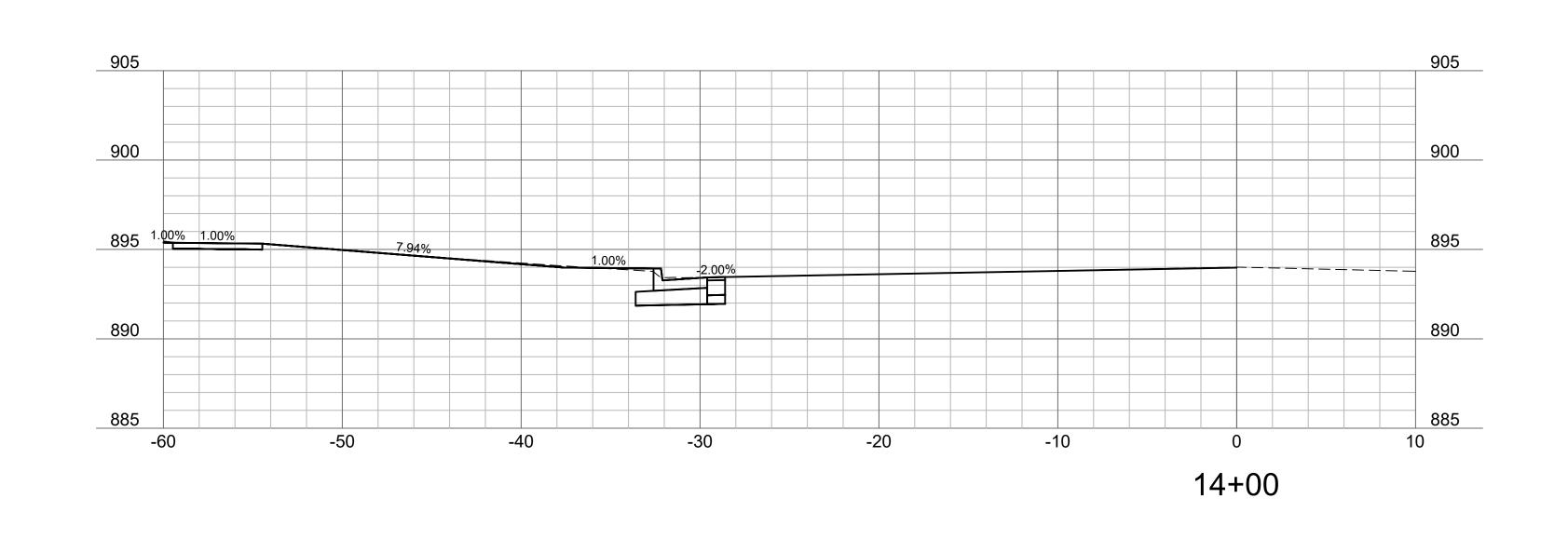
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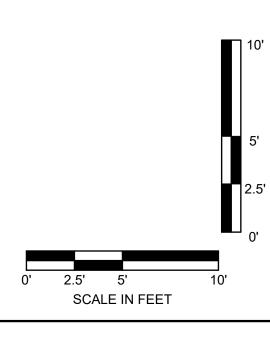
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SHEET 31 of 33













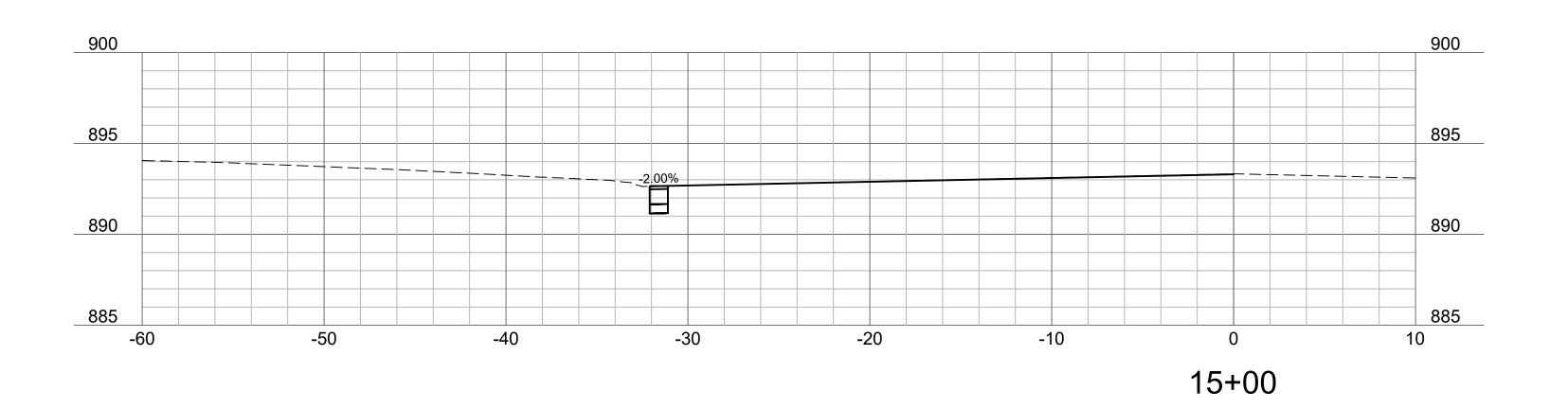
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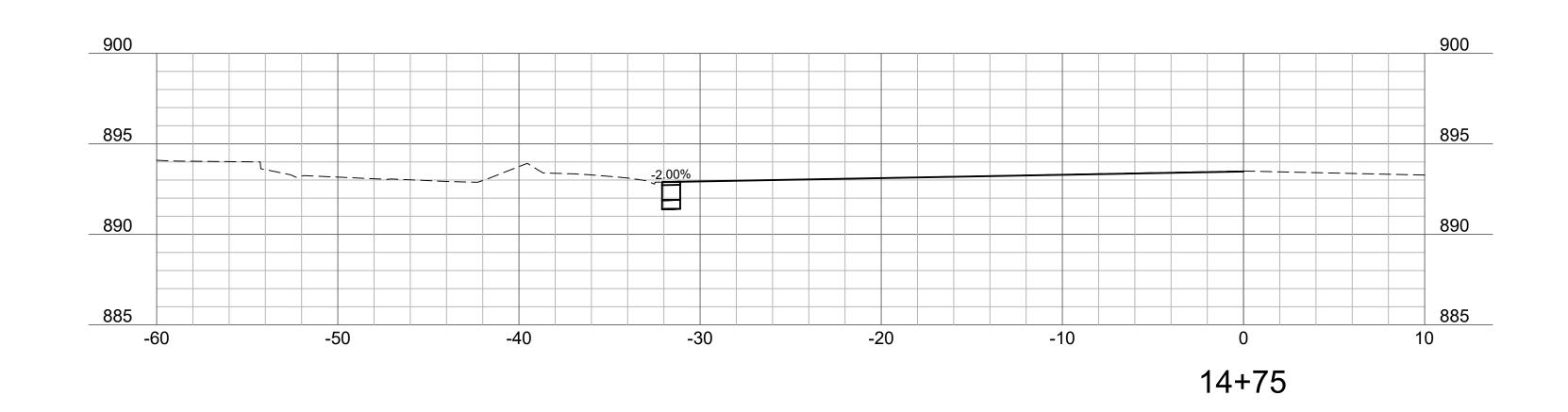
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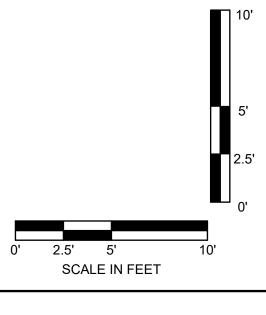
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